

Public Document Pack
SOUTHEND-ON-SEA BOROUGH COUNCIL

Development Control Committee

Date: Wednesday, 3rd June, 2020

Time: 5.00 pm

Place: Virtual Meeting via MS Teams

Contact: Tim Row - Principal Democratic Services Officer

Email: committeesection@southend.gov.uk

A G E N D A

1 Apologies for Absence

2 Declarations of Interest

3 Supplementary Report

**** **Contents List and Introduction**

4 **20/00311/BC3M - Car Park at Former Gas Works Site, Eastern Esplanade (Kursaal Ward) (Pages 5 - 36)**

5 **20/00328/BC3M - Southchurch Park East, Lifstan Way (Thorpe Ward) (Pages 37 - 62)**

6 **20/00655/FUL - 85 Western Road, Leigh-on-Sea (West Leigh Ward) (Pages 63 - 114)**

7 **20/00580/FUL - Morrison's, Western Approaches (St Laurence Ward) (Pages 115 - 140)**

TO: The Chairman & Members of the Development Control Committee:

Councillor N Ward (Chair)

Councillors M Borton (Vice-Chair), B Ayling, J Beck, A Chalk, D Cowan, A Dear, F Evans, D Garston, S Habermel, D Jarvis, A Jones, H McDonald, C Mulroney, A Thompson, S Wakefield and C Walker

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DEVELOPMENT CONTROL COMMITTEE

AGENDA: 3rd June 2020

| WARD | APP/REF NO. | ADDRESS |
|-------------|--------------------|----------------|
|-------------|--------------------|----------------|

| Main Plans Report | | |
|--------------------------|---------------|--|
| Kursaal | 20/00311/BC3M | Car Park At Former Gas Works Site Eastern Esplanade |
| Thorpe | 20/00328/BC3M | Southchurch Park East Lifstan Way |
| West Leigh | 20/00655/FUL | 85 Western Road Leigh-On-Sea |
| St Laurence | 20/00580/FUL | Morrison's Western Approaches |

DEVELOPMENT CONTROL COMMITTEE

INTRODUCTION

- (i) **Recommendations in capitals at the end of each report are those of the Corporate Director of Place, are not the decision of the Committee and are subject to Member consideration.**
- (ii) All plans have been considered in the context of the Borough Council's Environmental Charter. An assessment of the environmental implications of development proposals is inherent in the development control process and implicit in the reports.
- (iii) Reports will not necessarily be dealt with in the order in which they are printed.
- (iv) The following abbreviations are used in the reports:-

| | |
|-------------|--|
| BLP | - Borough Local Plan |
| DAS | - Design & Access Statement |
| DEFRA | - Department of Environment, Food and Rural Affairs |
| DPD | - Development Plan Document |
| EA | - Environmental Agency |
| EPOA | - Essex Planning Officer's Association |
| DCLG | - Department of Communities and Local Government |
| NPPF | - National Planning Policy Framework |
| NPPG | - National Planning Practice Guidance |
| SPD | - Supplementary Planning Document |
| SSSI | - Sites of Special Scientific Interest. A national designation. SSSIs are the country's very best wildlife and geological sites. |
| SPA | - Special Protection Area. An area designated for special protection under the terms of the European Community Directive on the Conservation of Wild Birds. |
| Ramsar Site | - Describes sites that meet the criteria for inclusion in the list of Wetlands of International Importance under the Ramsar Convention. (Named after a town in Iran, the Ramsar Convention is concerned with the protection of wetlands, especially those important for migratory birds) |

Background Papers

- (i) Planning applications and supporting documents and plans
- (ii) Application worksheets and supporting papers
- (iii) Non-exempt contents of property files
- (iv) Consultation and publicity responses
- (v) NPPF and NPPG
- (vi) Core Strategy
- (vii) Borough Local Plan

NB Other letters and papers not taken into account in preparing this report but received subsequently will be reported to the Committee either orally or in a supplementary report.

DEVELOPMENT CONTROL COMMITTEE

Use Classes

- Class A1 - Shops
- Class A2 - Financial & Professional Services
- Class A3 - Restaurants & Cafes
- Class A4 - Drinking Establishments
- Class A5 - Hot Food Take-away

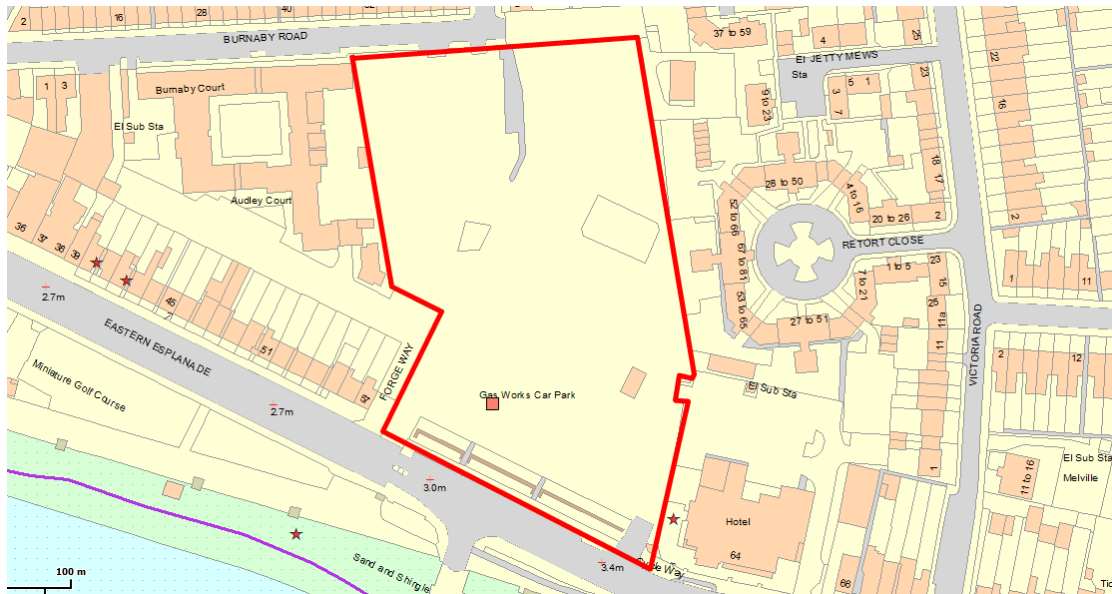
- Class B1 - Business
- Class B2 - General Industrial
- Class B8 - Storage or Distribution

- Class C1 - Hotels
- Class C2 - Residential Institutions
- Class C3 - Dwellinghouses
- Class C4 - Small House in Multiple Occupation

- Class D1 - Non-Residential Institutions
- Class D2 - Assembly and Leisure
- Sui Generis - A use on its own, for which any change of use will require planning permission

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|-----------------------------|--|------------|
| Reference: | 20/00311/BC3M | <h1>4</h1> |
| Application Type: | Borough Council Regulation 3-Major | |
| Ward: | Kursaal | |
| Proposal: | Resurface existing car park and layout additional coach and car parking | |
| Address: | Car Park At Former Gas Works Site, Eastern Esplanade, Southend-On-Sea | |
| Applicant: | Southend On Sea Borough Council | |
| Agent: | Sharon Harrington of Southend Borough Council | |
| Consultation Expiry: | 19th March 2020 | |
| Expiry Date: | 21st May 2020 | |
| Case Officer: | Charlotte White | |
| Plan Nos: | MW192020 (Location Plan), MW,1920202 (Block Plan), Design and Access Statement, MW,192020 Resurfacing Plan. | |
| Recommendation: | GRANT PLANNING PERMISSION, subject to conditions | |



1 Site and Surroundings

- 1.1 The site constitutes a previous gas works site. The site has since been cleared and is currently, lawfully used as a car park, with temporary planning permission granted under reference 18/00634/BC3M to use the vacant site as a temporary car park for a period of 5 years. The existing car park has separate points of access and egress from Eastern Esplanade.
- 1.2 The site has a frontage to both Eastern Esplanade (to the south) and Burnaby Road (to the north). The surrounding area is mixed in character with residential dwellings, hotels and commercial units in the wider surrounding area.
- 1.3 The site is located in flood zone 3a. There is a Conservation Area to the west of the site (Eastern Esplanade Conservation Area) which includes Grade II Listed Buildings and locally listed buildings. Within the Southend Central Area Action Plan (SCAAP) (2018) the site is located within the Central Seafront Policy Area. The SCAAP identifies the area to the front of the site as a key public realm improvement area and part of the seafront landmark area.

2 The Proposal

- 2.1 Planning permission is sought to resurface the northern/rear part of the car park with black asphalt and layout additional coach and car parking. There are currently 176 car parking spaces on the site including 8 disabled spaces and 10 motorcycle spaces and 19 coach parking spaces. It is proposed to provide an additional 83 parking spaces, resulting in a total of 259 spaces on the site. No additional motorcycle spaces are proposed (10 retained), an additional 6 disabled spaces are proposed (resulting in a total of 14 disabled spaces). An additional 8 coach spaces are proposed, resulting in a total of 27 coach spaces on site (although it is noted that the applicant has stated that up to 40 coaches could be parked on the site, with the car park being used in a flexible way).
- 2.2 The Design and Access Statement submitted indicates that the access to the car park will be via the existing entrance and exit from Eastern Esplanade. Pedestrian access is step free. The coach parking will be operated as a pre-book car park but with flexibility to accommodate pay and display as required.
- 2.3 The Design and Access Statement submitted states that the proposal is to resurface the existing hard standing area with black asphalt, layout formal car and coach parking and provide ANPR (Automatic Number Plate Recognition) security systems. It is stated that the car park gates will be closed overnight at 10pm, except on the days of main organised events such as the Seafront Fire Work Displays, albeit, the information submitted indicates that the car park did close at 10pm during the firework displays at the end of last year. It is stated that the existing lighting columns will be retained which have adjustable control of lux (i.e. luminance) levels and are on a timer. It is stated that the existing landscaping will be retained to the southern boundary of the site with further planting undertaken where appropriate. The Design and Access Statement states that it is proposed to reuse the existing site drainage with the new car park surface laid to a slight fall to shed water to the existing drains.

- 2.4 The application has been submitted with a Design and Access Statement, Noise Impact Assessment, Geo-Environmental Investigation, Air Quality Assessment, Transport Assessment and Flood Risk Assessment.

3 Relevant Planning History

- 3.1 The site has a fairly extensive planning history. The most relevant planning history is outlined below:
- 3.2 18/00634/BC3M - Use vacant site as temporary car park (for a period of five years) and widen existing entrance onto Eastern Esplanade, changes to hard and soft landscaping, boundary treatments and associated works – Temporary planning permission granted.
- 3.3 13/00869/EXTM - Hybrid Application to demolish the existing buildings, erect mixed development comprising 216 flats, 64 bedroom hotel, restaurant and retail floor space in 4, 5, 7 and 12 storey blocks with piazza, semi-underground and surface car parks, cycle parking, associated infrastructure (Full Application) and use the land fronting Burnaby Road for affordable housing (Outline Application) (Application to extend the time limit for implementation of 10/00140/FULM granted on 16/08/2010) – Allowed at appeal.
- 3.4 10/00140/FULM - Hybrid Application to demolish the existing buildings, erect mixed development comprising 216 flats, 64 bedroom hotel, restaurant and retail floor space in 4, 5, 7 and 12 storey blocks with piazza, semi-underground and surface car parks, cycle parking, associated infrastructure (Full Application) and use the land fronting Burnaby Road for affordable housing (Outline Application) – Planning permission granted.

4 Representation Summary

4.1 Public Consultation

150 neighbouring properties were consulted, a site notice displayed and the application was advertised in the press. 3 letters of representation have been received which make the following summarised comments:

- Concerns relating to coaches parking/stopping in the entrance to the Gas Works car park and road and blocking access to Audley Court.
- Concerns relating to the entrance and exit of the car park.
- Increase in rubbish.
- Concerns relating to lack of landscaping and trees and benches
- Uneven and dangerous footpaths.
- Concerns relating to the flood lights – flood lights directed to apartments in Retort Close, making sleeping impossible.
- Concerns that coaches leave their engines running whilst parked, and request to limit diesel exhaust.

4.2 Cadent

There is apparatus in the vicinity which may be affected by the activities specified.

4.3 Highways Team

No objections raised

4.4 **Parks**

The development site is located approximately 35m from the Southend Marshes SSSI and Ramsar Site. There should be no negative impact to the surrounding environment which includes Southend beaches and marshes. Request a condition that landscaping is agreed before opening the site.

4.5 **Council's SuDS Engineers**

Requests additional information.

Officer comment: A condition has been recommended in this respect.

4.6 **Environmental Health**

Raises no objection – recommends conditions relating to construction and contamination.

4.7 **Environment Agency**

No objection.

5 **Planning Policy Summary**

5.1 The National Planning Policy Framework (NPPF) (2019)

5.2 Core Strategy (2007): Policies KP1 (Spatial Strategy), KP2 (Development Principles), KP3 (Implementation and Resources), CP3 (Transport and Accessibility), CP4 (The Environment and Urban Renaissance) and CP6 (Community Infrastructure)

5.3 Development Management Document (2015): Policies DM1 (Design Quality), DM2 (Low Carbon and Efficient Use of Resources), DM3 (Efficient and Effective Use of Land), Policy DM5 (Southend-on-Sea's Historic Environment) Policy DM6 (The Seafront), DM14 (Environmental Protection) and DM15 (Sustainable Transport Management)

5.4 Southend Central Area Action Plan (SCAAP) (2018) Policies DS3 (Landmarks and Landmark Buildings), DS4 (Flood Risk Management and Sustainable Drainage), DS5 (Transport, Access and Public Realm) and CS1 (Central Seafront Policy Area Development Principles).

5.5 Design & Townscape Guide (2009)

5.6 CIL Charging Schedule (2015)

6 **Planning Considerations**

6.1 The main considerations in relation to this application are the principle of the development, design and impact on adjoining heritage assets, traffic and transportation matters, impact on residential amenity and the environment, flood risk and drainage considerations and CIL (Community Infrastructure Levy). The planning history is a material consideration in the determination of the application.

7 Appraisal

Principle of Development

- 7.1 Paragraph 117 of the NPPF states: Planning policies and decisions should promote an effective use of land in meeting the need for homes and other uses, whilst safeguarding and improving the environment and ensuring safe and healthy living conditions. Strategic policies should set out a clear strategy for accommodating objectively assessed needs, in a way that makes as much use as possible of previously-developed or 'brownfield' land. Policy KP2 of the Core Strategy requires *'all new development...make the best use of previously developed land, ensuring that sites and buildings are put to best use.'* Policy CP4 of the Core Strategy similarly seeks to maximise *'the use of previously developed land'*.
- 7.2 Policy KP1 of the Core Strategy states *'appropriate regeneration and growth will be focused in the...seafront to enhance the Seafront's role as a successful leisure and tourist attraction and place to live, and make the best use of the River Thames, subject to the safeguarding of the biodiversity importance of the foreshore.'*
- 7.3 Policy CP3 of the Core Strategy states that *'improvement to transport infrastructure and services will be sought...by widening travel choice, particularly by car share, rail, bus, including social transport, taxi, cycling and walking...'*
- 7.4 Policy DS5 of the SCAAP states *'In order to support the vitality and viability of the SCAAP area the Council will maintain parking capacity within Southend Central Area at a level that supports vitality and viability and does not undermine the Central Area's ability to accommodate visitor trips, whilst enabling the delivery of relevant opportunity sites.'*
- 7.5 Policy DS4 of the SCAAP states development proposals which are within a flood zone *'will be accompanied by a flood risk assessment...will locate more vulnerable uses in the areas of the proposal least at risk...will achieve an appropriate degree of safety over the lifetime of the development...'* Policy DM6 of the Development Management Document similarly requires developments in this location to take account of flood risk.
- 7.6 The site has previously been granted planning permission to be redeveloped to provide a mixed development including residential units, hotel accommodation and commercial uses. However this permission has now lapsed. More recently temporary planning permission was granted on 6th June 2018 for a period of 5 years to use the site as a car park. This temporary permission has been implemented and remains in place until 6th June 2023. The front of the site has already been re-surfaced and laid out for car parking. The rear of the site is currently being used for coach parking. This application seeks to resurface and formalise the parking to the rear of the site, with parking provided for cars and coaches, with some flexibility. As such the principle of using the site for parking purposes has already been considered acceptable for a temporary period. The use would provide parking for members of the public and for coaches which would benefit tourism/day trips and would benefit other uses and businesses in the surrounding area and the wider community. No objection is therefore raised to the principle of the temporary use proposed in this respect, subject to a similar condition, restricting the use for a further 5 years from the date of this permission.

- 7.7 The site is located within Flood Zone 3a (high probability of flooding). The application has been submitted with a Flood Risk Assessment and Drainage Strategy which comments that the proposed use constitutes a 'less vulnerable use' which is an appropriate use within flood zone 3a. The Flood Risk Assessment also identifies that the site benefits from flood defences. The Flood Risk Assessment concludes that the development does not increase flood risk either on site or elsewhere and that the proposal will remain safe for the lifetime of the development. Given the findings of this report and given the nature and temporary use of the proposed development no objection is raised to the principle of the development on this basis, Flood risk considerations are considered in greater detail below. It is also noted that no objection was previously raised on this basis under reference 18/00634/BC3M.
- 7.8 This proposal is considered in more detail in the context of the policies relating to design and impact on the adjoining Heritage Assets below. However, no objection is raised on this basis in principle.
- 7.9 There is therefore no objection to the principle of using the site as a surface car park for a temporary period of 5 years, subject to the scheme satisfying each of the considerations detailed below.

Design and Impact on the Character of the Area and Adjoining Heritage Assets

- 7.10 Section 72(1) of the Planning and Listed Building and Conservation Areas Act 1990 states that special attention should be paid to the desirability of preserving or enhancing the character or appearance of the Conservation Area and Section 66(1) of the same Act states for development which affects a Listed Building or its setting that special regard shall be had to the desirability of preserving the building or its setting or any feature of special architectural interest that it possesses.
- 7.11 It should be noted that good design is a fundamental requirement of new development to achieve high quality environments. Its importance is reflected in the National Planning Policy Framework, in Policies KP2 and CP4 of the Core Strategy and in Policy DM1 of the Development Management Document. The Design and Townscape Guide also states that "*the Borough Council is committed to good design and will seek to create attractive, high-quality living environments.*"
- 7.12 Paragraph 124 of the NPPF states '*The creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.*'
- 7.13 Policy DM1 of the Development Management Document states that all development should "*add to the overall quality of the area and respect the character of the site, its local context and surroundings in terms of its architectural approach, height, size, scale, form, massing, density, layout, proportions, materials, townscape and/or landscape setting, use, and detailed design features*".
- 7.14 Paragraph 193 of the NPPF states '*When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be).*'

- 7.15 Paragraph 195 of the NPPF states *‘Where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss if necessary to achieve substantial public benefits that outweigh that harm or loss...’*
- 7.16 Paragraph 196 of the NPPF states *‘Where a development proposal will lead to less than substantial harm to the significance or a designated asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.’*
- 7.17 Policy DM5 of the Development Management Document states *‘Development proposals that result in the total loss of or substantial harm to the significance of a designated heritage asset, including listed buildings and buildings within conservation areas, will be resisted, unless there is clear and convincing justification that outweighs the harm or loss. Development proposals that are demonstrated to result in less than substantial harm to a designated heritage asset will be weighed against the impact on the significance of the asset and the public benefits of the proposal, and will be resisted where there is no clear and convincing justification for this.’*
- 7.18 Policy DS5 of the SCAAP states *‘In order to support the vitality and viability of the SCAAP area the Council will ensure new and existing car parks add to the overall aesthetic quality of an area through such measures as landscaping, green walls, public art, pedestrian walkways and pedestrian permeability, as well incorporating layouts to reduce visual impact and effect on key views within and to Southend Central Area.’*
- 7.19 The proposed car park is of a relatively utilitarian design, however, the existing site has already been granted temporary planning permission to be used as a car park. The scheme does include some small areas of landscaping which is positive. It is noted that there have already been improvements to the appearance of the site since the granting of temporary planning permission in 2018 with, for example the introduction of gabion walls and trees to the front of the site, in accordance with Policy DS5 of the SCAAP. As such, on balance, the proposal is considered acceptable and policy compliant in the above regards.
- 7.20 The site is located adjacent to a Conservation Area; the Eastern Esplanade Conservation Area which constitutes numbers 40-57 Eastern Esplanade; a row of mid-19th Century terraced cottages. No’s 40-45 constitute Grade II Listed Buildings and numbers 46-57 constitute locally listed buildings. Given that temporary planning permission has already been granted for the use of the site as a car park, given that landscaping improvements have already occurred and that some additional landscaped areas are proposed as part of this application, it is considered that the proposed development would preserve the character and appearance of the adjoining heritage assets, in accordance with National and Local Planning Policies.
- 7.21 It is considered that the development would be of an acceptable overall design that would not result in any material harm to the visual amenity of the area. The development would preserve the character and appearance of the adjoining Conservation Area and would not result in any material harm to the setting of the nearby Listed Buildings. The proposal is therefore acceptable and policy compliant in this respect.

Impact on Residential Amenity

- 7.22 Policies DM1 and DM3 of the Development Management Document and CP4 of the Core Strategy refer to the impact of development on surrounding occupiers. High quality development, by definition, should provide a positive living environment for its occupiers whilst not having an adverse impact on the amenity of neighbours. Protection and enhancement of amenity is essential to maintaining people's quality of life and ensuring the successful integration of proposed development into existing neighbourhoods.
- 7.23 Policy DM14 of the Development Management Document states that *'Development on or near land that is known to be contaminated or which may be affected by contamination will only be permitted where an appropriate contaminated land assessment has been carried out...where contamination is found which would pose an unacceptable risk to people's health, the natural environment or water quality, the Council will impose a condition, if appropriate, to ensure the applicant undertake remedial measures to ensure that the site is suitable for the proposed use and that the development can safely proceed. Remediation works will be carried out before the commencement of any new development'*.
- 7.24 Given the nature of the proposed development, the proposal would not result in any material harm to the living conditions of the adjoining residents in terms of overlooking, loss of privacy, dominance, an overbearing impact, loss of light and outlook, overshadowing or a material sense of enclosure.
- 7.25 Given the nature of the proposal, the development has the potential to result in noise and disturbance to the adjoining residents. In this respect, the application has been submitted with a noise impact assessment which concludes *'calculations indicate that the ambient sound level including the operation of the car park is likely to remain unchanged during the daytime hours suggesting no observed effect at the nearest noise sensitive receptors. As such, the site is therefore considered to be suitable for the proposed use with regard to noise and no additional mitigation measures are recommended'*.
- 7.26 In this respect, it is noted that Burnaby Road to the rear of the site has a more residential nature and is quieter than Eastern Esplanade to the front of the site. However, it is proposed, as existing, that vehicles will access and egress the site from Eastern Esplanade. This is a positive element of the proposal which will significantly reduce its impact. Eastern Esplanade is already a busy road and as such it is not considered that the proposed use would result in any material harm to the adjoining residents fronting Eastern Esplanade in terms of noise and disturbance over and above the existing situation. The site also adjoins dwellings to the east and west, however, it is apparent that fencing has been installed to the boundaries of the site which would provide some sound attenuation. It is also noted that there are some existing parking courts serving the flats to the immediate east and west of the site which directly adjoin the application site and that the information submitted with this application indicates that the gates to the car park will be closed at 10pm (except during main organised events). However, it is noted that the information submitted by the application indicates that during the fire work displays last year, the car park was still closed at 10pm. Temporary planning permission for the use of the site for parking purposes for up to 318 cars and 27 coach parking spaces has already been granted and does not expire until 6th June 2023.

The Environmental Health Team has raised no objection on this basis. The proposal is considered to be acceptable and policy compliant in regards to potential noise and disturbance impacts in the round.

- 7.27 In terms of lighting, the application seeks to retain the existing lighting columns and as such it is considered that the proposal would not result in any material harm to the adjoining occupiers in terms of light pollution over and above the existing situation. A condition can be imposed requiring the external lighting to be directed, sited and screened away from the adjacent properties. Subject to such a condition no objection is raised on this basis. It is also noted that Environmental Health have raised no objection on this basis and that temporary planning permission has already been granted to use this site for parking purposes.
- 7.28 This application has been submitted with an air quality assessment which concludes *'It is considered that the effect of the development on air quality will be insignificant to minor adverse and therefore the proposal fully accords with the principals of the AQMA. In conclusion, there would be no materially significant additional air quality impacts to the vicinity and sensitive receptors arising as a result of the proposal for the car park...'* In this respect, it is noted that the Environmental Health raised no objection on this basis. No objection is raised to the scheme in this respect. A representation received has raised concerns that coach engines are left switched on whilst parked which causes fumes and nuisance. A condition can be imposed on any grant of consent in this respect.
- 7.29 As such it is considered that the development would not result in any material adverse harm to the residential amenity of the adjoining residents. The proposal is therefore policy compliant in this respect.
- 7.30 In terms of land contamination, the application has been submitted with a Geo-Environmental Investigation Report. This report comments that the site formerly housed the Southend Gas Works, infrastructure associated with the gas works including gas holders, tar and liquid tanks and above ground oil storage tanks were previously located across the site. The report states that no significant sources of on-going contamination were noted during the site walkover.
- 7.31 The report concludes that elevated levels of high molecular weight organic contamination were noted in the groundwater and slightly elevated levels of mobile organics were observed, however, none of the remedial targets were exceeded by the maximum measured concentrations recorded in the groundwater retrieved from beneath the site and as such remedial action to afford protection to the Estuary receptor would not be required. Whilst localised contaminated soils were noted, the concentrations encountered did not exceed the relevant guidelines based on the proposed use of the site. Furthermore the site is to be covered with hardstanding thus severing any source receptor pathways. Therefore any risks to human health through contaminant uptake are considered to be low. The report recommends that clean top soil and sub soil are imported into any proposed soft landscaped areas. The report recommends a geotextile marker layer below any imported materials in this respect. The risk from the very small quantities of asbestos encountered are considered to be low, especially as the proposal seeks to cap the entirety of the site with hardstanding. Any services should be protected if included in the redevelopment of the site. No gas protection measures are necessary as no new buildings are proposed on the site.

The report submitted therefore concludes that a watching brief be implemented on this site and should any further contamination be discovered all works should cease and a suitably qualified person should attend the site to agree a formal remediation strategy. The report's final conclusions are that based on the proposed redevelopment of the site as a car park, provided that the recommended remedial works are implemented that the site would not be considered to be 'contaminated land'.

- 7.32 In this respect, Environmental Health has raised no objection to the principle of the development, recommending a condition in terms of contamination. A condition can be attached to any grant of consent in this respect. Environmental Health also recommends a condition restricting the construction times which is considered reasonable. However, given the nature of the development proposed and the coverage of these matters under other legislation it is not considered necessary to impose conditions requiring dust emission details or restricting the burning of waste materials on the site.
- 7.33 As such, subject to conditions the development is considered acceptable and policy compliant and no objection is raised on this basis.

Traffic and Transportation Issues

- 7.34 Policy DM15 of the Development Management Document states that *'Development will be allowed where there is, or it can be demonstrated that there will be physical and environmental capacity to accommodate the type and amount of traffic generated in a safe and sustainable manner...access to the proposed development and any traffic generated must not unreasonably harm the surroundings...'*
- 7.35 Paragraph 109 of the NPPF states *'Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.'*
- 7.36 This planning application has been submitted with a Transport Assessment which concludes that the results of the trip generation modelling demonstrates that the proposed development trips can be accommodated within Eastern Esplanade and will not have a negative impact on Eastern Esplanade and the impact of the car park will not be severe – which is the requirement of the NPPF, as outlined above.
- 7.37 No changes are proposed to the access. The existing access and egress are considered acceptable, providing good visibility in both directions and therefore do not harm highway safety.
- 7.38 The layout and design of the car park is acceptable. The highways team has commented that it is not considered that the application will have a detrimental impact on the highway or the surrounding areas and that the detailed information submitted with the application is robust.
- 7.39 The development is therefore acceptable and policy compliant in this regard and no objection is therefore raised on this basis.

Flood Risk and Drainage

- 7.40 Policy KP1 of Core Strategy states that all development proposals within flood risk zones “*shall be accompanied by a detailed flood risk assessment appropriate to the scale and the nature of the development and the risk*”. It is also noted that “*development will only be permitted where that assessment clearly demonstrates that it is appropriate in terms of its type, siting and the mitigation measures proposed, using appropriate and sustainable flood risk management options.*”
- 7.41 Policy DS4 of the SCAAP requires development proposals located in flood zones to be ‘*accompanied by a flood risk assessment...locate more vulnerable uses in the areas of the proposal at least risk...achieve an appropriate degree of safety over the lifetime of the development...*’
- 7.42 Paragraph 155 of the NPPF states ‘*Inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk (whether existing or future). Where development is necessary in such areas, the development should be safe for its lifetime without increasing flood risk elsewhere.*’
- 7.43 Paragraph 158 of the NPPF states ‘*The aim of the sequential test is to steer new development to areas with the lowest risk of flooding. Development should not be allocated or permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower risk of flooding.*’
- 7.44 Paragraph 159 of the NPPF states ‘*If it is not possible for development to be located in zones with a lower risk of flooding (taking into account wider sustainable development objectives), the exception test may have to be applied. The need for the exception test will depend on the potential vulnerability of the site and of the development proposed, in line with the Flood Risk Vulnerability Classification set out in national planning guidance.*’
- 7.45 The Guidance contained within the Planning Practice Guide states ‘*The Sequential Test does not need to be applied for...applications for minor development or change of use (except for a change of use to a caravan, camping or chalet site, or to a mobile home or park home site)*’
- 7.46 The site is located within Flood Zone 3a; the high probability flood zone, however, the proposed use is considered to constitute a less vulnerable use. In this respect, the Environment Agency’s Flood Risk Vulnerability Classification table identifies less vulnerable development as appropriate within flood zone 3a. The Environment Agency previously raised no objection to a similar proposal under reference 18/00634/BC3M on flood risk grounds and have not raised an objection to this proposal on flood risk grounds. The Environment Agency state they have no objection on flood risk grounds provided the local planning authority is satisfied that the sequential test is passed and the development would be safe for its lifetime. In this respect, the Environment Agency comment that long-term and residential car parking is unlikely to be acceptable in areas that regularly flood due to the risk of car owners being away from the area and being unable to move their cars when a flood occurs. The Environment Agency also commented that the site owner should be signed up to the public flood warning service and that signage should be installed and evacuation procedures should be in place.

- 7.47 The application has been submitted with a Flood Risk Assessment (FRA) which concludes that the sequential test does not need to be applied for changes of use and notes that whilst the site is located within Flood Zone 3a, the existing flood defences currently protect the area providing a standard of protection between 1 in 99 and 1 in 1000 years and the risk of flooding from other sources is low. The FRA also concludes that the development would not increase flood risk on the site or elsewhere and the site will remain safe for the lifetime of the development.
- 7.48 The FRA submitted also makes a commitment to sign up to the Environment Agency's Floodline Warning Direct Services, that the applicant will monitor the weather following advanced warnings from the Environment Agency of a storm approaching and will manage the operation of the car park such that the public are not put at risk in the event of the flood defences being breached. It is also noted that there is no proposal for nearby residents to use this car park with the car park locked from later evening onwards.
- 7.49 As such, given the findings and commitments within the FRA submitted and given the Environment Agency comments received, subject to a condition requiring the development to be undertaken in accordance with the approved FRA no objection is raised to the development in terms of Flood Risk. It is also noted that there was no objection raised previously in this respect under reference 18/00634/BC3M.
- 7.50 In terms of surface water management the FRA submitted indicates that the existing surface water system will continue to serve the application site. The report concludes that during an extreme rainfall event (in excess of the 1 in 100 year event plus climate change), the surface water drainage network capacity may become overwhelmed resulting in surface flooding and overland flows. The FRA therefore states that the operation and maintenance of the development's drainage infrastructure needs to be agreed with the key stakeholders.
- 7.51 The Council's SuDS Engineers have commented that additional information is required to satisfy planning requirements. The following information has been identified as being required; a drainage strategy, greenfield runoff rates, existing runoff rates during storm events, details of how the drainage satisfies the SuDS techniques in terms of water quality and attenuation and discharge quantity, clarification of overland flow routes, connection points and proposed method of flow control, SuDS clarification, a system of valuation, provision of drainage for large storm events, details of conveyance and exceedance flow routes, details of SuDS management and evidence of consent from Southend Council that they will be responsible for private surface water drains. Following the provision of such information, the SuDS Engineers indicate that planning conditions could be imposed requiring the applicant to provide evidence of infiltration testing, a method statement regarding the management of surface water runoff during the construction phase, evidence of consent from Anglian Water for the proposed discharge rate and connection location to the public sewer and details of the management and maintenance for all SuDS and how they will be secured for the lifetime of the development.
- 7.52 Given that temporary planning permission has already been granted to use the site as car park (reference 18/00634/BC3M), in this instance, it is considered that conditions can be imposed on any grant of consent requiring the above details to be submitted to and approved by the Local Planning Authority. Subject to such a condition the development is considered acceptable and policy compliant in this respect.

- 7.53 The Environment Agency has raised no objection to the proposal. Given the nature of the proposal which includes limited alterations to the site and given the temporary nature of the proposal no objection is raised to the scheme on flooding or drainage grounds, subject to conditions.

Community Infrastructure Levy (CIL)

- 7.54 As the proposed development equates to less than 100sqm of new floorspace, and does not involve the creation of a new dwelling (Class C3), the development benefits from a Minor Development Exemption under the Community Infrastructure Levy Regulations 2010 (as amended) and as such no charge is payable.

8 Conclusion

- 8.1 Having regard to all material considerations it is considered that, subject to compliance with the attached conditions, the proposed development would be acceptable and compliant with the objectives of the relevant local development plan policies and guidance as well as those contained within the National Planning Policy Framework. The site already benefits from temporary planning permission to be used as a car park until 6th June 2023. There is no objection to the principle of the development, the design or impact on the character and appearance of the site or the wider surrounding area. The development would preserve the character and appearance of the nearby Conservation Area and would not materially harm the setting of the nearby listed buildings. The proposed would not materially harm residential amenity, is acceptable from a highways and transportation perspective and subject to conditions is acceptable in flood terms. This application is therefore recommended for approval, subject to conditions.

9 Recommendation

- 9.1 Members are recommended to GRANT TEMPORARY PLANNING PERMISSION subject to the following conditions:**

- 01 The development hereby permitted shall begin no later than three years from the date of this decision.**

Reason: Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

- 02 The permission hereby approved is for a temporary period of 5 years from the date of the permission only. After 5 years from the date of the granting of this permission, the use hereby approved shall cease.**

Reason: For the avoidance of doubt and in accordance with the National Planning Policy Framework (2019) and Policies KP1 and KP2 of the Core Strategy (2007).

- 03 The development hereby permitted shall be carried out in accordance with the following approved plans: MW192020 (Location Plan), MW,1920202 (Block Plan), MW,192020 Resurfacing Plan.**

Reason: To ensure that the development is carried out in accordance with the provisions of the Development Plan.

04 Before any vehicle parking permitted within the scope of this approval takes place, a strategy for implementation of the hardsurfacing works including timescales, phasing, any mitigation necessary and implementation of the recommendations set out in the strategy shall be submitted to and approved in writing by the local planning authority. The strategy shall include, but not be limited to the following matters:

- Hard and soft landscaping
- Drainage infrastructure
- Contamination/remediation
- Car park management arrangements

The development shall be undertaken in accordance with the approved strategy in perpetuity.

Reason: To safeguard the character and appearance of the surrounding area and the amenities of neighbouring occupiers, to ensure satisfactory drainage of the site, in accordance with National Planning Policy Framework (2019), Policies DM1, DM3, DM5 and DM14 of the Development Management Document (2015) and Policies KP2 and CP4 of the Core Strategy (2007).

05 The development hereby permitted shall not be carried out except in complete accordance with the approved Flood Risk Assessment undertaken by BdR, reference 18-0073 dated 28th February 2018, including the flood risk management recommendations on page 12 unless otherwise previously agreed in writing by the local planning authority.

Reason: To ensure the development is safe and to ensure compliance with the National Planning Policy Framework (2019) and Core Strategy (2007) Policies KP1 and KP2.

06 The development hereby permitted shall not be carried out except in complete accordance with the approved Geo-environmental Investigations undertaken by Ground and Environmental Services Limited, reference 11691-1 dated January 2018, including the recommendations outlined on pages 39-41 unless otherwise previously agreed in writing by the local planning authority.

Reason - To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors, to ensure compliance with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1, DM3 and DM14 and the advice contained within the Design and Townscape Guide (2009).

07 Prior to the installation of any geotextile membrane on the site, details of the material to be used, including full manufacturer's specifications shall be submitted to and approved in writing by the local planning authority. The development shall be undertaken in accordance with the approved details only and thereafter permanently retained as such in perpetuity.

Reason - To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors, to ensure compliance with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1, DM3 and DM14 and the advice contained within the Design and Townscape Guide (2009).

- 08 Any external lighting installed shall be directed, sited and screened away from the adjoining and nearby residential properties and retained as such in perpetuity.**

Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice contained within the Design and Townscape Guide (2009).

- 09 The development hereby approved shall only be undertaken between 8am and 6pm Monday to Friday and 8am to 1pm Saturdays and at no other time whatsoever.**

Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice contained within the Design and Townscape Guide (2009).

- 10 No closed circuit television (CCTV) shall be installed until full details of the CCTV have been submitted to and approved in writing by the local planning authority. The CCTV shall be implemented in accordance with the approved details.**

Reason: In the interests of the visual amenity of the surrounding area and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice within the Design and Townscape Guide (2009).

- 11 Rear access to Burnaby Road shall only be use in an emergency and at no other times whatsoever.**

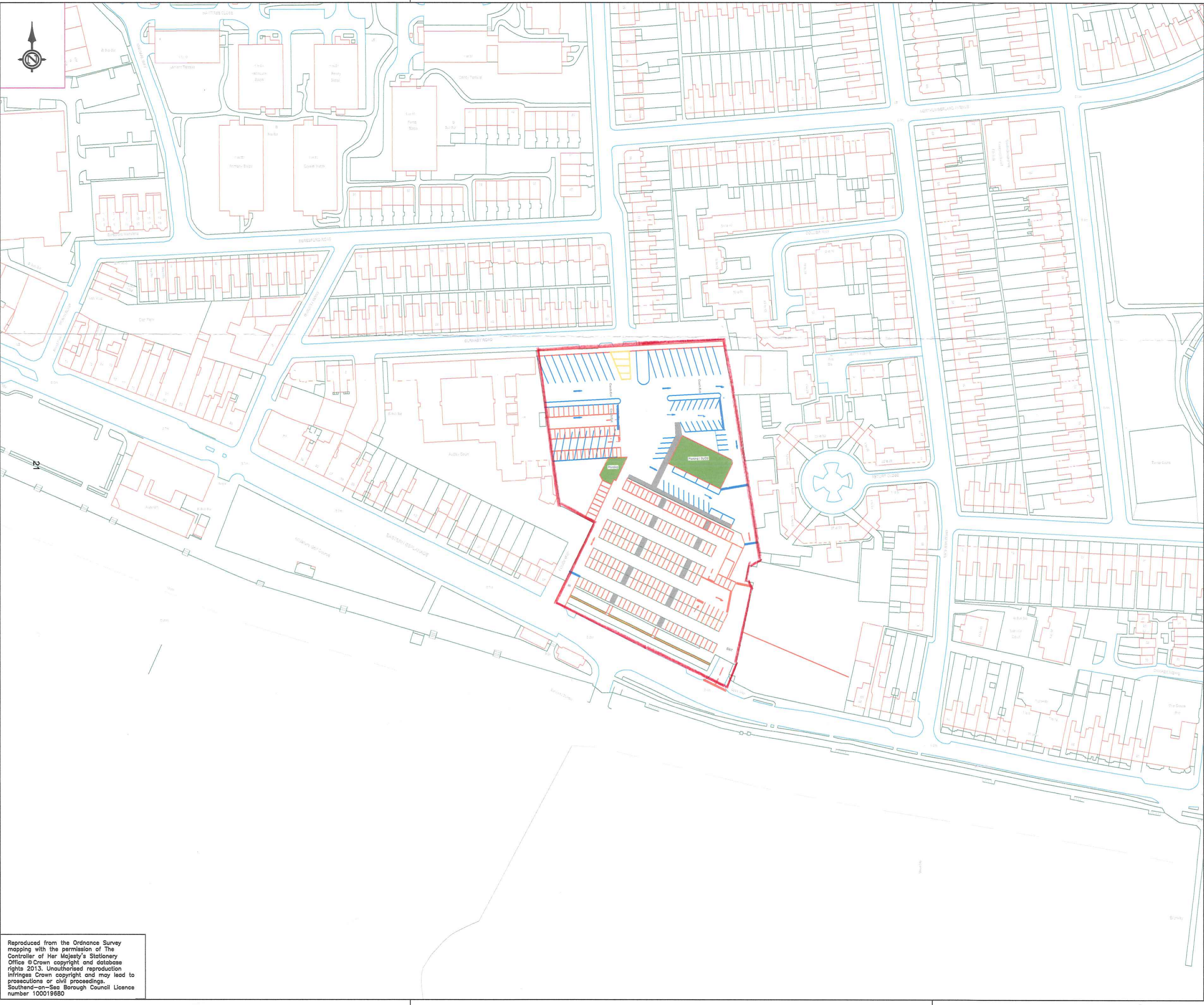
Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, within the Design and Townscape Guide (2009).

No coaches shall be parked within the application site with their engines left running.

Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, within the Design and Townscape Guide (2009).

Informatives:

- 01** You are advised that as the proposed extension(s) to your property equates to less than 100sqm of new floorspace the development benefits from a Minor Development Exemption under the Community Infrastructure Levy Regulations 2010 (as amended) and as such no charge is payable. See www.southend.gov.uk/cil for further details about CIL.
- 02** You should be aware that in cases where damage occurs during construction works to the highway in implementing this permission that Council may seek to recover the cost of repairing public highways and footpaths from any party responsible for damaging them. This includes damage carried out when implementing a planning permission or other works to buildings or land. Please take care when carrying out works on or near the public highways and footpaths in the borough.
- 03** The applicant is reminded that this permission does not bestow compliance with other regulatory frameworks. In particular your attention is drawn to the statutory nuisance provisions within the Environmental Protection Act 1990 (as amended) and also to the relevant sections of the Control of Pollution Act 1974. The provisions apply to the construction phase and not solely to the operation of the completed development. Contact 01702 215005 for more information.
- 04** If it is predicted that the construction works are likely to cause a nuisance the applicant should apply for a prior consent application under section 61 of the Control of Pollution Act 1974. A consent enables the applicant to conduct the works without the worry of enforcement provided they comply with it. The applicant will have to submit details of any noisy works including type of plant and machinery to be used, proposed daily start and finish times, consultation with nearby residents and businesses and duration and time scales of the works. The applicant should contact the Regulatory Services Team at Southend-on-Sea Borough Council for details.



| Revisions | | |
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**Southend-on-Sea
Borough Council**

Neighbourhoods &
Environments

PO Box 5560, Civic Centre
Victoria Avenue, Southend on Sea,
SS2 6ZQ

Quality Project No. :

Project Title

**Car Park
Improvements**

Drawing Title

Gasworks

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| Drawn | Date | 1:1250 | |
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Drawing Status

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| Preliminary | <input checked="" type="checkbox"/> | Working | <input type="checkbox"/> |
| Tender | <input type="checkbox"/> | As Constructed | <input type="checkbox"/> |

Drawing Number
MW192020

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Southend-on-Sea Borough Council

Neighbourhoods & Environments
 PO Box 5560, Civic Centre
 Victoria Avenue, Southend on Sea,
 SS2 6ZQ

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| Quality Project No. : | |
| Project Title | |
| Car Park Improvements | |
| Drawing Title | |
| Gasworks | |
| Designed by | Scales © A2 |
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| Approved | Date |
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| Drawing Status | |
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| Preliminary | X Working |
| Tender | As Constructed |

Drawing Number
MW,192020



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Gas Works Car Park

by Charlotte White



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COACHES

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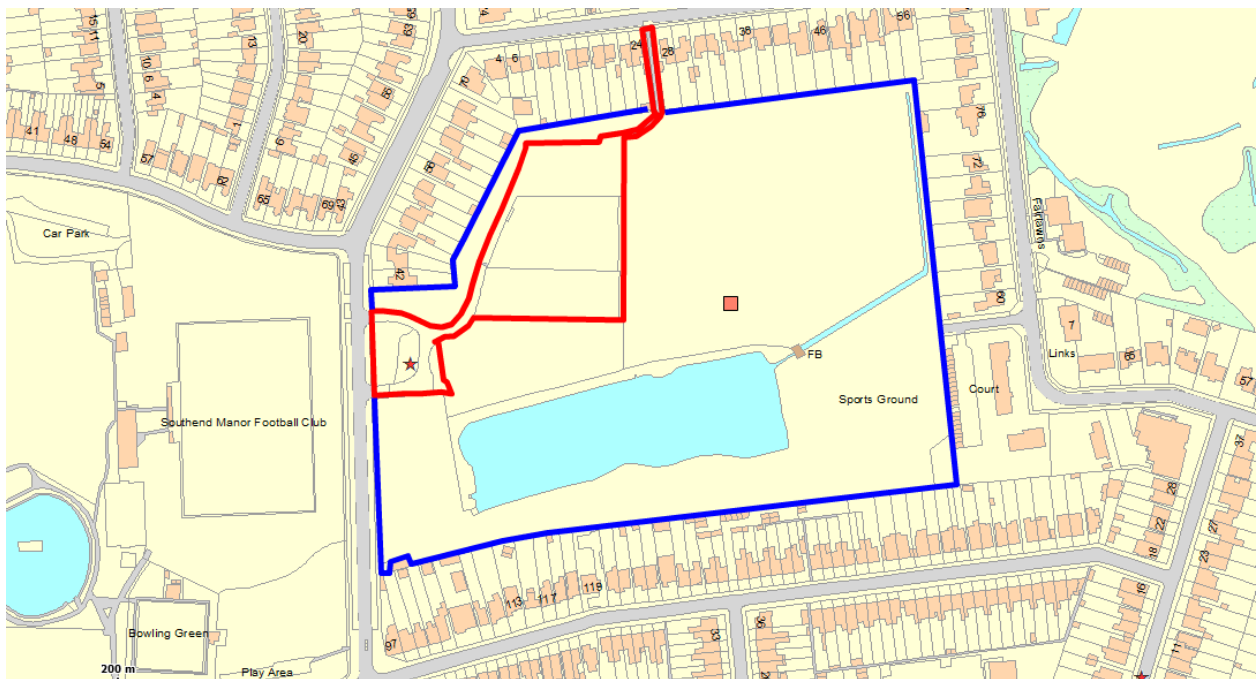
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|-----------------------------|--|------------|
| Reference: | 20/00328/BC3M | <h1>5</h1> |
| Application Type: | Borough Council Regulation 3-Major | |
| Ward: | Thorpe | |
| Proposal: | Resurface existing car park and layout additional coach and car parking | |
| Address: | Southchurch Park East, Lifstan Way, Southend-on-Sea, Essex | |
| Applicant: | Southend On Sea Borough Council | |
| Agent: | Sharon Harrington of Southend Borough Council | |
| Consultation Expiry: | 24th March 2020 | |
| Expiry Date: | 25 th May 2020 | |
| Case Officer: | Charlotte White | |
| Plan Nos: | MW4328 (Block Plan dated 01/03/2020), MW4328 (Site Location Plan dated 01/03/2020), Lifstan Way Current Layout Plan | |
| Recommendation: | GRANT PLANNING PERMISSION, subject to conditions | |



1 Site and Surroundings

- 1.1 The site is located to the east of Lifstan Way. It is irregular in shape and includes an existing hardsurfaced car park area, a gravel, informal parking area, a grassed area and the existing separate in and out access and egress to the site. There are residential dwellings to the west and north of the site. To the east of the site is an open space and to the south of the site is a pond.
- 1.2 Within the Development Management Documents Proposal's Map, the site is allocated as Protected Green Space. The site is mainly located within Flood Zone 3, with a small area to the north-west of the site within flood zone 2.

2 The Proposal

- 2.1 Planning permission is sought to resurface the existing informal, gravel parking area and layout additional flexible coach and car parking spaces. From the information provided it is evident that there are 67 parking spaces on site currently, including 5 disabled spaces. This proposal will increase this by an additional 157 spaces, providing a total of 224 parking spaces on the site, which includes an additional 5 disabled spaces. The proposal will also provide 11 coach parking spaces. The coach parking spaces constitute dual spaces, overlapping some of the car parking spaces, allowing the parking area to be used in a flexible way. The information submitted within the Design and Access Statement states that during times of coach use, the parking bays will be suspended directly behind the coaches to allow for vehicles to manoeuvre effectively.
- 2.2 The Design and Access Statement submitted confirms that the car park will be accessed via the existing entrance and exit from Lifstan Way. It is indicated that the car park will be resurfaced with a black asphalt finish, will provide a formal layout for car and coach parking and will provide ANPR (Automatic Number Plate Recognition) security systems. It is stated that the gates will be closed overnight, except on days of main organised events. The information in the design and access statement indicates that the existing lighting columns will be retained which have adjustable control of lux (i.e. luminance) levels and are on a timer. In terms of landscaping it is stated that the existing landscaping will be retained within the site, with further planting undertaken where appropriate. It is stated that the drainage strategy proposes to reuse the existing site drainage and that the new car park surface will be laid to a slight fall to shed water to the existing slot drains.
- 2.3 The application has been submitted with a Design and Access Statement.

3 Relevant Planning History

- 3.1 None

4 Representation Summary

4.1 Public Consultation

77 neighbouring properties were consulted, a site notice displayed and the application was advertised in the press.

23 letters of representation have been received which make the following summarised comments:

- Residential Amenity concerns.
- Concerns relating to the loss of open space and dog walking space and provision of car and coach parking in this location.
- Concerns relating to having to pay to park and the cost of parking and it resulting in additional parking in surrounding streets.
- Highway safety concerns.
- Insufficient space for coaches to manoeuvre
- Environmental and flooding concerns. Regular flooding in area and concerns relating to using tarmac in a flood plain.
- Has been attended by school and local residents for years – planting wild flowers and trees.
- Loss of view.
- Suggest trees and high scrub planted around southern edge of proposed coach parking to provide screening.
- Tourists being put above residents.
- Queries regarding whether any green space or trees will be lost and the total number of spaces proposed, the opening times, gates and security.
- Disturb wildlife and detrimental to flora, fauna and natural habitat including great crested newts. Lack of protected species surveys.
- Will impact quiet pursuits.
- Health and Safety concerns and safety concerns for children using the park.
- No risk assessment.
- Air Quality concerns
- No tree report submitted. Unclear if trees to be removed.
- Drainage concerns.
- Gas works car park already has enough parking.
- Should be encouraging walking not driving.
- Car park is used for school pick-up and drop-off therefore concerns to making the car park pay and display and that this will cause on-street parking and subsequent highway safety concerns.
- Suggestions relating to pay and display at certain times/or short term free for school drop off/pick up purposes.
- Tarmacking over a popular public green space and park.
- Pay and display will also deter dog walkers.
- Pollution and noise
- Regularly witness groups of scooters congregating in area at anti-social hours, when gates are locked, causing noise and disturbance. To reduce risk of this, suggest facility is secured accordingly and lighting and CCTV used to reduce risk.
- Concerns relating to traveller incursions, fly tipping and anti-social behaviour.
- Cost of upgrading the surface and of CCTV is unwarranted.
- Concerns that the proposal is linked to the Seaway proposals and comments that the car park is significantly removed from the Town Centre/Seafront, etc.
- Conservation Area concerns (*Officer comment: the site is not located within a Conservation Area*).
- Contrary to the zero carbon commitment.
- Representations have been received stating that there is a petition objecting to

this proposal. But the petition has not been submitted.

The comments made have been considered in the determination of the application. The matters are not found to represent a reasonable basis to refuse planning permission in the circumstances of this case.

4.2 **Sport England**

The proposed development does not fall within our statutory or non-statutory remit.

4.3 **Highways Team**

It is not considered that the application will have a detrimental impact upon the public highway in the surrounding areas. Opportunities to enhance the green space areas with additional landscape planting will be undertaken. The north part of the current car park area is used for staff parking for Greenways School and there are no plans to change this arrangement.

4.4 **Parks**

Landscape plan for the planting areas to remain to be agreed prior to construction. There should be no negative impact to the surrounding environment, including Southchurch Park and Southchurch Park East. Tree protection measures in accordance with BS5837 shall be implemented to protect the trees surrounding the site.

4.5 **Council's SuDS Engineers**

Requests additional information. (*Officer comment: A condition is recommended in this respect*).

4.6 **Environmental Health**

No objection subject to conditions relating to construction and external illumination.

4.7 **Environment Agency**

No objection subject to a condition requiring surface water draining from areas of hardstanding passing through an oil separator or a series of oil separators prior to being discharged into any watercourse, soakaway or surface water sewer.

4.8 **Fire Authority**

The arrangements should be in accordance with the details contained in the Approved Document to Building Regulation B5.

5 **Planning Policy Summary**

5.1 The National Planning Policy Framework (NPPF) (2019)

5.2 Core Strategy (2007): Policies KP1 (Spatial Strategy), KP2 (Development Principles), KP3 (Implementation and Resources), CP3 (Transport and Accessibility), CP4 (The Environment and Urban Renaissance), CP6 (Community Infrastructure) and CP7 (Sport, Recreation and Green Space).

5.3 Development Management Document (2015): Policies DM1 (Design Quality), DM2 (Low Carbon and Efficient Use of Resources), DM3 (Efficient and Effective Use of Land), DM14 (Environmental Protection) and DM15 (Sustainable Transport Management)

5.4 Design & Townscape Guide (2009)

5.5 CIL Charging Schedule (2015)

6 Planning Considerations

6.1 The main considerations in relation to this application are the principle of the development, design and impact on the character and appearance of the area, traffic and transportation matters, impact on residential amenity and ecology, the environment, flood risk and drainage considerations and CIL (Community Infrastructure Levy).

7 Appraisal

Principle of Development

7.1 Paragraph 117 of the NPPF states: Planning policies and decisions should promote an effective use of land in meeting the need for homes and other uses, whilst safeguarding and improving the environment and ensuring safe and healthy living conditions. Strategic policies should set out a clear strategy for accommodating objectively assessed needs, in a way that makes as much use as possible of previously-developed or 'brownfield' land. Policy KP2 of the Core Strategy requires *'all new development...make the best use of previously developed land, ensuring that sites and buildings are put to best use.'* Policy CP4 of the Core Strategy similarly seeks to maximise *'the use of previously developed land'*.

7.2 Policy KP1 of the Core Strategy states *'appropriate regeneration and growth will be focused in the...seafront to enhance the Seafront's role as a successful leisure and tourist attraction and place to live, and make the best use of the River Thames, subject to the safeguarding of the biodiversity importance of the foreshore.'*

7.3 Policy CP3 of the Core Strategy states that *'improvement to transport infrastructure and services will be sought...by widening travel choice, particularly by car share, rail, bus, including social transport, taxi, cycling and walking...'*

7.4 Paragraph 97 of the NPPF states *'Existing open space, sports and recreational buildings and land, including playing fields, shall not be built on unless:*

a) an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or

b) the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or

c) the development is for alternative sports and recreation provision, the benefits of which clearly outweigh the loss of the current or former use.'

7.5 Policy KP2 of the Core Strategy states that *'All new development, including transport infrastructure, should contribute to economic, social, physical and environmental regeneration in a suitable way...this must be achieved in ways which...respect, conserve and enhance and where necessary adequately mitigate effects on the natural and historic environment, including the Borough's biodiversity and green space resources, ensure that European and international sites for nature conservation are not adversely affected and contribute positively towards the 'Green Grid' in Southend.'*

7.6 Policy CP7 of the Core Strategy states *'The Borough Council will bring forward proposals that contribute to sports, recreation and green space facilities within the Borough for the benefit of local residents and visitors.'* The policy goes on to states *'All existing and proposed sport, recreation and green space facilities (including the Southend foreshore and small areas of important local amenity, community resources or biodiversity value) will be safeguarded from loss or displacement to other uses, except where it can clearly be demonstrated that alternative facilities of a higher standard are being provided in at least an equally convenient and accessible location to serve the same local community, and there would be no loss of amenity or environmental quality to that community.'*

7.7

The site is already being used for parking purposes, with a type 1/gravel finish and informal parking, with no spaces marked out. It was apparent from the site visit and from aerial photos that this site has been used for parking purposes for some time. In fact, the historical mapping from 1939 labels this general area as "car park" as shown below.



7.8 As such, whilst the site is allocated as protected green space the development which would not extend into the surrounding fields and which would formalise the existing gravel parking area would not result in any material loss of open space. It is also noted that the highways team have commented that opportunities to enhance the green space areas with additional landscape planting will be undertaken as part of the proposal. As such, given the location and extent of the proposal, the development is considered acceptable and policy compliant in principle including policies CP7.

7.9 The use would provide parking for members of the public and for coaches which would benefit tourism/day trips and would benefit other uses and businesses in the surrounding area and the wider community. No objection is therefore raised to the principle of the development in this respect.

7.10 The site is located within Flood Zone 3a (high probability of flooding). Whilst the application has not been submitted with a Flood Risk Assessment or Drainage Strategy, the proposed use of the site for car parking is considered to constitute a 'less vulnerable use' which is an appropriate use within flood zone 3a. Flood risk is considered in more detail below but given the nature of the use, no objection is raised to the principle of the development of this basis.

7.11 There is therefore no objection to the principle of the development.

Design and Impact on the Character of the Area

7.12 It should be noted that good design is a fundamental requirement of new development to achieve high quality environments. Its importance is reflected in the National Planning Policy Framework, in Policies KP2 and CP4 of the Core Strategy and in Policy DM1 of the Development Management Document. The Design and Townscape Guide also states that *“the Borough Council is committed to good design and will seek to create attractive, high-quality living environments.”*

7.13 Paragraph 124 of the NPPF states *‘The creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.’*

7.14 Policy DM1 of the Development Management Document states that all development should *“add to the overall quality of the area and respect the character of the site, its local context and surroundings in terms of its architectural approach, height, size, scale, form, massing, density, layout, proportions, materials, townscape and/or landscape setting, use, and detailed design features”.*

7.15 The proposed car park is of a relatively utilitarian in design, however given that the existing area is already used as an informal car park with a gravel-type finish and given that it will be seen in the context of the existing tarmac car park at the site, on balance, it is considered that the development is of an acceptable overall design. The highways department has confirmed its commitment to providing landscaping improvements which is positive and can be secured via planning conditions.

7.16 The proposal is acceptable and policy compliant in these regards.

Impact on Residential Amenity

7.17 Policies DM1 and DM3 of the Development Management Document and CP4 of the Core Strategy refer to the impact of development on surrounding occupiers. High quality development, by definition, should provide a positive living environment for its occupiers whilst not having an adverse impact on the amenity of neighbours. Protection and enhancement of amenity is essential to maintaining people's quality of life and ensuring the successful integration of proposed development into existing neighbourhoods.

7.18 Policy DM14 of the Development Management Document states that *‘Development on or near land that is known to be contaminated or which may be affected by contamination will only be permitted where an appropriate contaminated land assessment has been carried out...where contamination is found which would pose an unacceptable risk to people’s health, the natural environment or water quality, the Council will impose a condition, if appropriate, to ensure the applicant undertake remedial measures to ensure that the site is suitable for the proposed use and that the development can safely proceed. Remediation works will be carried out before the commencement of any new development’.*

- 7.19 Given the nature of the proposed development, the proposal would not result in any material harm to the living conditions of the adjoining residents in terms of overlooking, loss of privacy, dominance, an overbearing impact, loss of light and outlook, overshadowing or a material sense of enclosure.
- 7.20 Given the nature of the proposal, the development has the potential to result in noise and disturbance to the adjoining residents. However, given that the site is already used for parking, in a more informal manner, and given that the coach parking is positioned away from the nearest dwellings (approximately 50m from the rear boundaries and some 85m from the rear elevation of the nearest adjoining dwellings), on balance it is considered that the development would not result in material noise and disturbance over and above the existing situation. It is also noted that the Environmental Health Team has raised no objection in this regard and that the information submitted indicates that the gates to the car park will be closed overnight. As such the development is considered acceptable in this instance.
- 7.21 In terms of lighting, the application seeks to retain the existing lighting columns and as such it is considered that the proposal would not result in any materially different impact to the adjoining occupiers in terms of light pollution over and above the existing situation. Environmental Health has recommended a planning condition is imposed requiring full details of the lighting. Subject to such a condition no objection is raised on this basis.
- 7.22 In terms of air quality, given that the site is already used for parking purposes and this development would provide a more formal parking layout, it is considered that the development would not result in any material harm to air quality over and above the existing situation.
- 7.23 As such it is considered that the development would not result in any material harm to the residential amenity of the adjoining residents. The proposal is therefore policy compliant in this respect.

Ecology

- 7.24 Paragraph 170 of the NPPF states that *'Planning policies and decisions should contribute to and enhance the natural and local environment by...protecting and enhancing valued landscapes, sites of biodiversity or geological vale and soils (in a manner commensurate with their statutory status or identified quality in the development plan).*
- 7.25 Paragraph 175 of the NPPF states *'When determining planning applications, local planning authorities should apply the following principles:*
- a) if significant harm to biodiversity resulting from a development cannot be avoided (through location on an alternative site with less harmful impacts), adequately mitigated, or, as a last resort, compensated for, then planning permission should be refused;*
- b) development on land within or outside a Site of Special Scientific Interest, and which is likely to have an adverse effect on it (either individually or in combination with other development), should not normally be permitted.*

The only exception is where the benefits of the development in the location proposed clearly outweigh both its likely impact on the features of the site that make it of special scientific interest, and any broader impacts on the national network of Sites of Special Scientific Interest.'

- 7.26 Whilst the site is located relatively close to a Local Wildlife Site, RAMSAR and SSSI, given the nature of the proposal and its existing use as a car park, it is considered that the development would not result in any material harm to ecology or biodiversity. The application is acceptable and policy compliant in this respect.

Traffic and Transportation Issues

- 7.27 Policy DM15 of the Development Management Document states that *'Development will be allowed where there is, or it can be demonstrated that there will be physical and environmental capacity to accommodate the type and amount of traffic generated in a safe and sustainable manner...access to the proposed development and any traffic generated must not unreasonably harm the surroundings...'*
- 7.28 No changes are proposed to the existing access and egress to the site. The existing access and egress is considered acceptable and it is noted that this proposal seeks to formalise the existing parking on the site. The parking arrangements allow for the flexible use of this space and are of an appropriate and acceptable layout. As such it is considered that the development would not result in any material harm to highway safety. It is also noted that the Highways Team have not raised an objection to the proposal, commenting that the application will not have a detrimental impact upon the public highway in the surrounding area.
- 7.29 It is noted that a number of neighbour concerns have been raised regarding the impact of the proposal on the existing car park that is used by a local school. However, it is noted that the highways team, in their consultation response has confirmed that the current car park area for staff parking for Greenways School is not sought to be altered.
- 7.30 The development is therefore acceptable and policy compliant in this regard and no objection is therefore raised on this basis.

Flood Risk and Drainage

- 7.31 Policy KP1 of Core Strategy states that all development proposals within flood risk zones *"shall be accompanied by a detailed flood risk assessment appropriate to the scale and the nature of the development and the risk"*. It is also noted that *"development will only be permitted where that assessment clearly demonstrates that it is appropriate in terms of its type, siting and the mitigation measures proposed, using appropriate and sustainable flood risk management options."*
- 7.32 Paragraph 155 of the NPPF states *'Inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk (whether existing or future). Where development is necessary in such areas, the development should be safe for its lifetime without increasing flood risk elsewhere.'*

- 7.33 Paragraph 158 of the NPPF states *'The aim of the sequential test is to steer new development to areas with the lowest risk of flooding. Development should not be allocated or permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower risk of flooding.'*
- 7.34 Paragraph 159 of the NPPF states If it is not possible for development to be located in zones with a lower risk of flooding (taking into account wider sustainable development objectives), the exception test may have to be applied. The need for the exception test will depend on the potential vulnerability of the site and of the development proposed, in line with the Flood Risk Vulnerability Classification set out in national planning guidance.'
- 7.35 The Guidance contained within the Planning Practice Guide states *'The Sequential Test does not need to be applied for...applications for minor development or change of use (except for a change of use to a caravan, camping or chalet site, or to a mobile home or park home site)*
- 7.36 The site is located within Flood Zone 3a; the high probability flood zone, however, the proposed use is considered to constitute a less vulnerable use. In this respect, the Environment Agency's Flood Risk Vulnerability Classification table identifies less vulnerable development as appropriate within flood zone 3a. The Environment Agency has not raised an objection to the proposal.
- 7.37 On balance, given that the site is already used for car parking and this proposal seeks to provide a more formal car and coach parking area, subject to conditions requiring the applicant to sign up to the Environment Agency's Floodline Warning Direct Service and subject to the applicant managing the operation of the car park appropriately in this respect to ensure that the public are not put at risk in the event of a flood and given that the car park will be closed over-night, the proposal is considered acceptable in flood risk terms.
- 7.38 In terms of surface water management, no drainage strategy has been provided. The Council's SuDS Engineers require the applicant to provide a drainage strategy. In this instance it is considered that the requirements of the SuDS Engineers can be addressed by the imposition of planning conditions. Subject to such conditions the proposal is considered acceptable and policy compliant in the above regards. The condition recommended by the Environment Agency in respect of oil separators can also be imposed on any grant of permission.

Community Infrastructure Levy (CIL)

- 7.39 As the proposed development equates to less than 100sqm of new floorspace, and does not involve the creation of a new dwelling (Class C3), the development benefits from a Minor Development Exemption under the Community Infrastructure Levy Regulations 2010 (as amended) and as such no charge is payable.

8 Conclusion

- 8.1 Having regard to all material considerations it is considered that, subject to compliance with the attached conditions, the proposed development would be acceptable and compliant with the objectives of the relevant local development plan policies and guidance as well as those contained within the National Planning Policy Framework. The site is already used for informal parking. There is no objection to the principle of the development, the design or impact on the character and appearance of the site or the wider surrounding area. The proposed would not materially harm residential amenity, is acceptable from a highways and transportation perspective and subject to conditions is acceptable in flood terms. This application is therefore recommended for approval, subject to conditions.

9 Recommendation

- 9.1 **Members are recommended to GRANT PLANNING PERMISSION subject to the following conditions:**

- 01 **The development hereby permitted shall begin no later than three years from the date of this decision.**

Reason: Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

- 02 **The development hereby permitted shall be carried out in accordance with the following approved plans: MW4328 (Block Plan dated 01/03/2020), MW4328 (Site Location Plan dated 01/03/2020), Lifstan Way Current Layout Plan**

Reason: To ensure that the development is carried out in accordance with the provisions of the Development Plan.

- 03 **Prior to first occupation of the development hereby approved, full details of the hard and soft landscape works to be carried out at the site shall have been submitted to and approved in writing by the local planning authority. All planting in the approved landscaping scheme shall be carried out in the first available planting season following the first use of the development hereby approved. Any trees or shrubs dying, removed, being severely damaged or becoming seriously diseased within five years of planting shall be replaced with trees or shrubs of such size and species as may be agreed with the Local Planning Authority. Hard landscaping shall be implemented in full accordance with the approved scheme prior to the first occupation of the development hereby approved. The details submitted shall include:**

- i. proposed finished levels or contours;**
- ii. hard surfacing materials and means of enclosing the site (including elevations of any boundary treatments);**
- iii. details of the number, size and location of the trees, shrubs and plants to be planted together with a planting specification and details of the management of the landscaping site.**

Reason: To safeguard the character and appearance of the surrounding area and the amenities of neighbouring occupiers in accordance with Policies DM1, DM3 and DM5 of the Development Management Document (2015) and Policies KP2 and CP4 of the Core Strategy (2007).

- 04 Prior to the first use of the development hereby approved, full details of the drainage infrastructure and a drainage strategy shall be submitted to and approved in writing by the local planning authority. The approved drainage infrastructure and strategy shall be implemented in full accordance with the approved scheme prior to the first use of the development hereby approved and be retained as such thereafter.**

Reason: To ensure satisfactory drainage of the site in accordance with Policy KP2 of the Core Strategy (2007) and Development Management Document (2015) Policy DM14.

- 05 Prior to the first use of the development hereby approved, the applicant shall sign up to the Environment Agency's Floodline Warning Direct Service and shall be retained as such in perpetuity.**

Reason: To ensure the development is safe and to ensure compliance with the National Planning Policy Framework (2019) and Core Strategy (2007) Policies KP1 and KP2.

- 06 No external lighting shall be installed unless and until details of all external illumination of the site including the luminance and spread of light and the design and specification of the light fittings has been submitted to and approved in writing by the local planning authority. All illumination shall be designed in accordance with the Institute of Lighting Professionals "Guidance Note 01/20: Guidance notes for the reduction of obtrusive light". All illumination within the site shall be retained in accordance with the approved details. There shall be no other lighting of the external areas of the site.**

Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice contained within the Design and Townscape Guide (2009).

- 07 Construction works associated with the development hereby approved shall only be undertaken between 8am and 6pm Monday to Friday and 8am to 1pm Saturdays and at no other time whatsoever.**

Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice contained within the Design and Townscape Guide (2009).

- 08 No closed circuit television (CCTV) shall be installed until full details of the CCTV have been submitted to and approved in writing by the local planning authority. The CCTV shall be implemented in accordance with the approved details.**

Reason: In the interests of the visual amenity of the surrounding area and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice within the Design and Townscape Guide (2009).

- 09 Notwithstanding the information submitted and otherwise approved, the development hereby approved shall not be first used unless and until a strategy setting out in full the management arrangements for the car park hereby approved, including the hours of operation, has been submitted to and approved in writing by the local planning authority. The approved management plan shall be implemented within prior to the first use of the development hereby approved.**

Reason: In the interests of the residential amenity of the adjoining residents and to ensure that the development complies with the National Planning Policy Framework (2019), Core Strategy (2007) Policies KP2 and CP4, Development Management Document (2015) Policies DM1 and DM3 and the advice within the Design and Townscape Guide (2009).

- 10 Notwithstanding the information submitted with the application, no development shall be undertaken unless and until tree protection measures in accordance with British Standard BS 5837 have been implemented to protect the trees surrounding the development site and these shall be retained throughout the development.**

Reason: To safeguard the character and appearance of the surrounding area in accordance with policies DM1 and DM3 of the Development Management Document (2015) and Policies KP2 and CP4 of the Core Strategy (2007).

- 11 Surface water draining from the areas of hardstanding hereby approved shall be passed through an oil separator or a series of oil separators prior to being discharged into any watercourse, soakaway or surface water sewer. The separator(s) shall be designed and constructed to have a capacity compatible with the area being drained, shall be installed prior to the occupation of the development and shall thereafter be retained and maintained throughout the lifetime of the development. Clean roof water or vehicle wash downs and detergents shall not pass through the separator(s) and should be drained instead to foul sewer or sealed systems.**

Reason: To reduce the risk of pollution and to ensure satisfactory drainage of the site in accordance with Policy KP2 of the Core Strategy (2007) and Development Management Document (2015) Policy DM14.

Informatives:

- 01** You are advised that as the proposed extension(s) to your property equates to less than 100sqm of new floorspace the development benefits from a Minor Development Exemption under the Community Infrastructure Levy Regulations 2010 (as amended) and as such no charge is payable. See www.southend.gov.uk/cil for further details about CIL.
- 02** You should be aware that in cases where damage occurs during construction works to the highway in implementing this permission that Council may seek to recover the cost of repairing public highways and footpaths from any party responsible for damaging them. This includes damage carried out when implementing a planning permission or other works to buildings or land. Please take care when carrying out works on or near the public highways and footpaths in the borough.
- 03** The applicant is reminded that this permission does not bestow compliance with other regulatory frameworks. In particular your attention is drawn to the statutory nuisance provisions within the Environmental Protection Act 1990 (as amended) and also to the relevant sections of the Control of Pollution Act 1974. The provisions apply to the construction phase and not solely to the operation of the completed development. Contact 01702 215005 for more information.
- 04** If construction works are to be considered outside of normal hours especially overnight it is recommended that the applicant applies for a prior consent application under section 61 of the Control of Pollution Act 1974. A consent enables the applicant to conduct the works without the worry of enforcement provided they comply with it. The applicant will have to submit details of any noisy works including type of plant and machinery to be used, proposed daily start and finish times, consultation with nearby residents and businesses and duration and time scales of the works. The applicant should contact the Regulatory Services Team at Southend-on-Sea Borough Council for details.

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Southchurch Park East

26-2-2020

by Charlotte White



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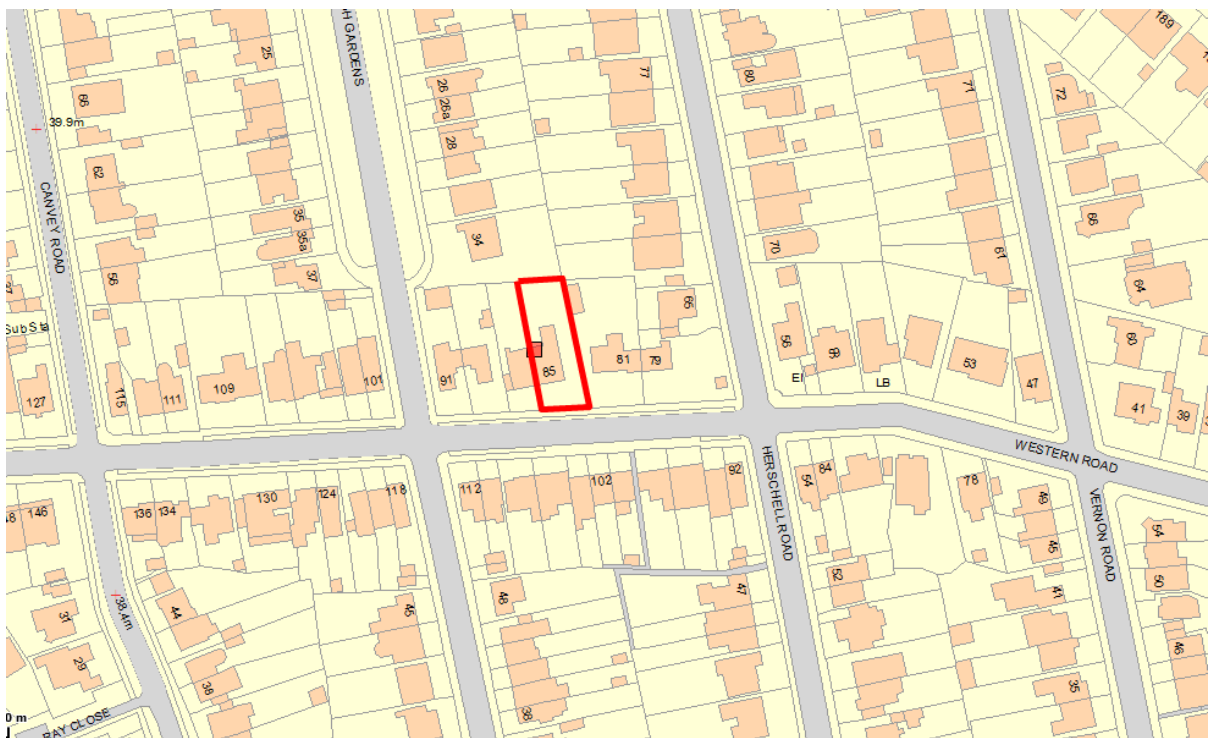
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|-----------------------------|---|------------|
| Reference: | 20/00655/FUL | <h1>6</h1> |
| Application Type: | Full Application | |
| Ward: | West Leigh | |
| Proposal: | Erect two storey side extension and part two storey first floor and single storey rear and side extension, to form two additional self-contained flats, lay out parking to front, communal amenity space, cycle and refuse stores to rear, alter elevations and form additional vehicular access onto Western Road (Amended Proposal) | |
| Address: | 85 Western Road, Leigh-On-Sea, Essex | |
| Applicant: | Mr Lake | |
| Agent: | DAP Architecture | |
| Consultation Expiry: | 29th May 2020 | |
| Expiry Date: | 22nd June 2020 | |
| Case Officer: | Robert Lilburn | |
| Plan Nos: | 905.001.00, 905.001.01, 905.002.01, 905.003.00, 905.004.00, 905.210.00, 905.211.00, 905.212.00, 905.213.00, 905.214.00, 905.215.00, 21208 se-01, 21208 se-02 | |
| Recommendation: | REFUSE PLANNING PERMISSION | |



1 Site and Surroundings

- 1.1 The application relates to a two-storey semi-detached building within a residential area. The submitted application form states that the existing use of the building is as a single dwelling, however this is updated with a subsequent description as two flats. The occupation as two self-contained flats appears from publicly available photographic records to be a long-standing arrangement of over four years' duration.
- 1.2 The submitted plans show that the existing flats each provide two bedrooms. They show that the ground floor flat provides two 2-person bedrooms and measures some 74sqm gross internal area (GIA). They also show that the first floor flat provides two bedrooms, of which one is a 2-person sized room, and one is 6.5sqm which is below the standard for a one-person bedroom further to the national Technical Housing Standards and is therefore considered a one-bedroom flat. The flat measures some 60sqm GIA.
- 1.3 The existing building is finished externally in white painted render and roof tiles. It has a single storey rear projection measuring approximately 5m in depth, with balustrades on top and the roof of the extension used as a balcony. It has a two-storey bay and decorative timber detailing to the gable above.
- 1.4 The frontage is open with no boundary wall and the space within the frontage has been block-paved for car parking. A fence separates the front and rear, and a rear garden of useable amenity space remains. Boundary treatments include low fences to the front (sides) and higher fencing to the rear garden. The site is relatively flat.
- 1.5 The extensive frontage block-paving, car parking and two main doors (one to the side) are the only obvious indications that the building is not a single dwelling, although these could equally be features of a dwelling house. As well as for the current two flats, there is no record of planning permission having been sought for the formation of a balcony at the rear or the block paving of the frontage. However, these arrangements appear to be long-standing. The character of the building is generally intact as that of a single dwelling and in that regard is wholly consistent with and comfortably situated in and complementary to its surroundings.
- 1.6 The immediate surroundings in Western Road are characterised predominantly by larger dwelling houses.

2 The Proposal

- 2.1 The application seeks planning permission for the extension and reconfiguration of the building to provide four self-contained flats.
- 2.2 The proposal incorporates a two-storey side extension which would measure some 3.7 m in width, half the width of the host semi-detached property, and would have a subservient roof ridge and eaves level relative to the existing building, dropped down at ridge level by some 0.5m. The front face of the side extension would be set back from the existing building by some 0.3m.
- 2.3 The proposal also incorporates a part two storey, part first floor, part single storey rear and side extension which would occupy the position of and extend beyond to both the rear and side of the existing, smaller, single storey rear projection.

- 2.4 The proposed extensions would have hipped, ridged roofs. The proposal also includes some reconfiguration of existing window openings and frames.
- 2.5 The applicant describes the proposed external materials as a white render finish. The submitted plans indicate that the roof would be finished in tiles similar in appearance to the existing.
- 2.6 To the rear of the building a bin store would be provided. The submitted plans show that it would accommodate four 240l wheeled bins. The store building would also provide enclosed accommodation for a cycle store providing space for four bicycles. The submitted plan shows that the bin and cycle store would be flat roofed, finished externally in larch timber cladding with a green roof and metal doors.
- 2.7 The submitted plan shows that following the development 128 square metres of communal amenity space would be available at the rear of the building equating to 43 square metres per flat.
- 2.8 The submitted plans show that Flat 1 at the ground floor rear of the building would measure some 63.6 square metres in gross internal area (GIA) with two bedrooms, accommodating 3 persons. The bedrooms would measure 11.6 square metres and 11.1 square metres respectively.
- 2.9 Flat 2 at the ground floor front of the building would measure some 49.7 square metres GIA with one bedroom, accommodating two persons. The bedroom would measure some 15.4 square metres.
- 2.10 Flat 3 at the first floor rear of the building would measure some 51.6 square metres GIA with one bedroom, accommodating two persons. The bedroom would measure some 12 square metres.
- 2.11 Flat 4 at the first floor front of the building would measure some 49.7 square metres GIA, with one bedroom, accommodating two persons. The bedroom would measure 15.1 square metres.
- 2.12 Four off-street car parking spaces would be provided to the front of the building in an area of hard standing with contrasting hard surfacing indicated on the submitted plans together with incidental soft landscaping to the edges.
- 2.13 It is noted that the submitted site layout plan incorrectly identifies the neighbouring property to the east as no.83; the property to the immediate east on Western Road is no.81 as shown on the location plan. This has not prejudiced the ability of the local planning authority to determine the application.
- 2.14 This application follows a previous application 18/02277/FUL which was refused. That application proposed to erect a two storey side extension and part ground/part first floor rear extension to form two additional self-contained flats, lay out parking to front, with communal amenity space, cycle and refuse stores to rear, alter rear elevation and extend vehicular access onto Western Road. It was refused for the following reasons:

- *The proposed side and rear extensions would by reason of their size and design be poorly integrated and overly dominant additions to the detriment of the character and appearance of the host property, the street scene and the surroundings including the rear garden scene. This would be unacceptable and contrary to the National Planning Policy Framework, Policies KP2 and CP4 of the Southend-on-Sea Core Strategy (2007), Policies DM1 and DM3 of the Southend-on-Sea Development Management Document (2015) and the advice contained within the Southend-on-Sea Design and Townscape Guide (2009);*
- *The proposed rear extension, as a result of its height, rear extent and proximity to site boundaries, would be a dominant, visually overbearing feature to neighbouring occupiers causing an undue sense of enclosure and overbearing impact. This would be harmful to neighbour amenities at nos.81 and 87 Western Road, unacceptable and contrary to the National Planning Policy Framework, Policies KP2 and CP4 of the Southend-on-Sea Core Strategy (2007), Policies DM1 and DM3 of the Southend-on-Sea Development Management Document (2015) and the advice contained within the Southend-on-Sea Design and Townscape Guide (2009);*
- *The high density form of the development would introduce the potential for a degree of comings and goings and general activity which would be harmful to the amenities of adjoining occupiers in an area characterised by a lower density of accommodation with significantly less activity. The harm caused by this more intense degree of activity in an extended semi-detached dwelling would be unacceptable and contrary to the National Planning Policy Framework, Policies KP2 and CP4 of the Southend-on-Sea Core Strategy (2007), Policies DM1 and DM3 of the Southend-on-Sea Development Management Document (2015) and the advice contained within the Southend-on-Sea Design and Townscape Guide (2009);*
- *The width of the proposed vehicular crossover would be excessive, and would reduce the extent of dedicated footway unacceptably, harming highway safety and compromising the safe and convenient passage of pedestrians, and the visual quality of the residential environment. This would be unacceptable and contrary to the National Planning Policy Framework, Policies KP2, CP3 and CP4 of the Core Strategy (2007), Policies DM1, DM3 and DM15 of the Development Management Document (2015) and the advice contained within the Design and Townscape Guide (2009).*

2.15 The key differences between the application under consideration and the earlier refused application are:

- The two storey side projection would be stepped back from the existing front elevation at both front elevation (by some 0.3m) and at roof level by some 0.5m at ridge;
- The two storey side and rear projection would be set in beyond the main rear elevation by some 0.7 m;
- The two storey element of the side and rear projection would be reduced to a rear of extent of some 4.3m from 6.1m;
- The two storey element of the side and rear projection would be increased in height from the previously proposed 4.8m at eaves level to some 5.4m;
- The frontage car parking spaces would be reconfigured so that the pedestrian

walkway would be positioned centrally rather than to the side;

- The proposed vehicular access would be divided in the middle by an island. A total width of the two crossings would consequently be some 0.8 m wider;
- The cycle store and refuse store would be reconfigured into an enclosed building;
- The proposed roof form would be reconfigured into a less bulky form more consistent with the existing building;
- The range of proposed accommodation would be altered from a previously proposed two x two bed 3-person flats and two x 1-bed two person flats to one x 2 bed three person flat and three x 1-bed two person flats;
- Windows and doors would be reconfigured and windows would be introduced on the east side elevation of the two-storey side projection.
- Two roof lights would be omitted.

2.16 The application has been called in to Development Control Committee by Councillor Evans.

3 Relevant Planning History

3.1 18/02277/FUL: Erect two storey side extension and part ground/part first floor rear extension to form two additional self-contained flats, layout parking to front, communal amenity space, cycle and refuse stores to rear, alter rear elevation and extend vehicular access onto Western Road (Amended Proposal). Refused.

3.2 18/01246/FUL: Erect two storey side and rear extension with access to rear parking, first and second floor rear extension to form three additional self-contained flats, install dormer to rear, refuse and cycle stores, canopy to front, alter elevations and extend vehicular access on to Western Road. Refused.

4 Representation Summary

Public Consultation

4.1 Nine neighbouring properties were notified and a site notice was posted. 15 letters of representation have been received from 13 addresses. Concerns raised are summarised as follows:

- Out of keeping with the character of the area;
- Effect on the sense of community;
- Cramped appearance and overdevelopment, overcrowding the site;
- Loss of privacy to neighbouring occupiers;
- Impacts on daylight and overshadowing to neighbouring occupiers;
- impacts of noise generated by occupiers;
- Overbearing appearance of proposed extensions;
- Impacts of refuse and cycle storage on neighbours;
- Loss of amenity to neighbours and wider area;
- Inadequate car parking for the development;
- Unsafe vehicular access;
- Traffic generation and impacts on highway safety;
- Impact of precedent for further development;
- Effect on house prices;
- Concern at processing of application during public health restrictions;
- Concern at repeated applications.

4.2 These concerns are noted and where relevant to material planning considerations they have been taken into account in the assessment of the application. Those remaining are found not to represent a reasonable basis to refuse planning permission in the circumstances of this case.

4.3 **Highways Team**

One off street car parking space has been provided for each flat. The vehicle crossovers that would serve the spaces are also policy compliant. The site is also in a sustainable location with regard to public transport. No highway objections are raised.

4.4 **Leigh Town Council**

Object to the application as the changes shown have been insufficient and previous objections remain. The proposal would, by reason of its design, size, bulk and mass, represent a cramped form of development and an overdevelopment of the land, which is out of keeping with and detrimental to the character and appearance of the application site and the area more widely.

5 Planning Policy Summary

5.1 The National Planning Policy Framework (NPPF) (2019)

5.2 Planning Practice Guidance and National Design Guide (2019)

5.3 Core Strategy (2007): Policies KP1 (Spatial Strategy) KP2 (Development Principles) CP3 (Transport and Accessibility) CP4 (Environment & Urban Renaissance) CP8 (Dwelling Provision)

5.4 Development Management Document (2015): Policies DM1 (Design Quality) DM2 (Low Carbon Development and Efficient Use of Resources) DM3 (The Efficient and Effective Use of Land) DM7 (Dwelling Mix) DM8 (Residential Standards) DM15 (Sustainable Transport Management)

5.5 Design & Townscape Guide (2009)

5.6 Vehicle Crossing Policy & Application Guidance (2014)

5.7 Waste Storage and Collection Guidance (2019)

5.8 National Housing Standards (2015)

5.9 CIL Charging Schedule (2015)

6 Planning Considerations

6.1 The main considerations in relation to this application are the principle of the development, design and impact on the character of the area, impact on neighbouring properties, living conditions for future occupiers, any traffic and transport issues and CIL, and whether the application has overcome the earlier reasons for refusal.

- 6.2 Since the application 18/02277/FUL was determined the revised National Planning Policy Framework (2019) came into force, replacing a previous version of the NPPF. It is considered that the contents of the revised NPPF do not materially alter the assessment of the impacts of the proposal.

7 Appraisal

Principle of Development

- 7.1 Government guidance contained within the National Planning Policy Framework (NPPF) encourages effective use of land (para.8) in particular previously developed land (para.117).
- 7.2 Policies KP1, KP2 and CP4 of the Core Strategy (2007) seek to promote sustainable development, including directing the siting of development through a sequential approach, minimising the use of 'greenfield' land.
- 7.3 Policy KP2 requires that new development contributes to economic, social, physical and environmental regeneration in a sustainable way through securing improvements to the urban environment through quality design and respecting the character and scale of the existing neighbourhood.
- 7.4 Policy CP4 seeks the creation of a high quality, sustainable urban environment which enhances and complements the natural and built assets of Southend. Policy CP4 requires that new development be of appropriate design and have a satisfactory relationship with surrounding development.
- 7.5 Policy CP8 requires that development proposals contribute to local housing needs. It identifies housing targets for Southend and seeks to protect the supply of valuable residential resources including the stock of large single family dwellings.
- 7.6 Policy DM1 seeks design quality that adds to the overall quality of an area and respects the character of a site and its local context. Policy DM3 of the Development Management Document (2015) seeks the efficient and effective use of land, provided it responds positively to local context and does not lead to over-intensification.
- 7.7 The Design and Townscape Guide seeks to promote a high quality of design in new developments. Para.80 of the Guide states that proposed accommodation mixes should reflect the local character.
- 7.8 Policy DM7 sets out a preferred market housing mix of 9% one-bed units, 22% two-bed units, 49% three-bed units and 20% four-bed units. Through Policy DM8 the Council seeks appropriate flexibility and dimensions within the internal accommodation to meet the changing needs of residents.
- 7.9 Policy DM15 states that development will be allowed where there is, or it can be demonstrated that there will be, physical and environmental capacity to accommodate the type and amount of traffic generated in a safe and sustainable manner.

- 7.10 The principle of extensions to the building is acceptable. The site is sustainably located in a reasonably accessible location. The proposal would add to the supply of housing within the urban area, re-using previously developed land. This would generally accord with the objectives of Policies KP1, KP2 and CP8 of the Core Strategy.
- 7.11 Single family dwellings are a unifying characteristic at this location and key to the local character. The existing flats at this location are in a pair of larger units with amenity space and landscaped setting commensurate with the local character, and as such may be regarded as providing family type accommodation. The proposal would continue to provide one unit of family sized accommodation albeit in a different configuration.
- 7.12 However, it would also be a higher density development introducing additional units of relatively small flats. The size of the flats are not such that would contribute to the greatest identified need in the Borough. The consequences of the higher density would be the potential imposition of design and amenity impacts and these are considered further below.
- 7.13 It is considered that the broad principle of residential development at this location is acceptable. However, a higher density development including small flats has the potential to harmfully impact the existing character and amenities, as well as altering the standard of living conditions available to occupiers of the building. Further detailed material planning considerations are discussed below.

Design and Impact on the Character of the Area

- 7.14 Paragraph 127 of the NPPF states that local planning authorities should aim to ensure that developments are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities).
- 7.15 Paragraph 130 of the NPPF states that “*permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions*”.
- 7.16 The National Design Guide seeks well-designed places in which development integrates and relates well into its surroundings.
- 7.17 The importance of good design is reflected in Policies KP2 and CP4 of the Core Strategy and also in Policies DM1 and DM3 of the Development Management Document. These policies seek to maintain and enhance the amenities, appeal and character of residential areas.
- 7.18 The Design and Townscape Guide also states that “*the Borough Council is committed to good design and will seek to create attractive, high-quality living environments*” and that “*proposed development [should] make a positive contribution to the local area*”.
- 7.19 At para.64 the Guide states that development should reflect the positive characteristics of its surroundings and extensions should integrate to their surroundings. Para.79 confirms the expectation that appropriate architectural language should be used reflecting the use of the building.

- 7.20 As underlined at para.80 of the Guide, the best use of land should be made without compromising quality of life. Para.85 of the Guide establishes that appropriate scale, height and massing are essential to the successful integration of new development. Para.115 of the Guide seeks cohesive design which responds positively to local context.
- 7.21 The existing building is a semi-detached form of building, and while the subject property is divided into two flats, its appearance is typically characteristic of family dwellings to be found in the area.
- 7.22 The principles underpinning the development plan policies and supplementary guidance relating to domestic extensions are considered relevant to this proposal.
- 7.23 The submitted plans show that the two-storey side extension would measure some 3.7m in width, while the existing semi-detached property is some 7.4m in width. This does not indicate a subservient scale of extension, particularly given the limited setting-back of 0.3m. The extension roof ridge would be dropped by 0.5m from the existing. The design of the extension including its roof form and the proportions of windows would be reasonably consistent with the host building. There would be space separating it from the neighbouring no.81 Western Road in the street scene and there would be no harmful terracing effect.
- 7.24 The limited stepping-back of the front elevation combined with the width of the side extension would not form an appropriately subservient extension. By virtue of this width the extension would have the harmful visual effect of being unduly dominant and would fail to respond appropriately to the existing building. This concern would not be overcome by the use of appropriately matching or contrasting materials, for example. The scale and design of the proposed rear element would be consistent with the scale, design and character of dwellings and buildings in the immediate surroundings. While the extensions would be significant in size, their design would reflect a simple domestic character. However, the scale of the proposed side projection is such that it would fail to integrate into the street scene appropriately. It would not be subservient to the parent building and would fail to respect the scale of the host building. The scale and resulting appearance of the development would thus be harmful to the visual amenities of the area.
- 7.25 The layout of the frontage, providing car parking on a hardstanding in front of the flats, is a negative aspect of the design. However, it would not be sufficiently different to the existing situation, excluding the likely additional cars and the proposed extended dropped kerb, to warrant a reason for refusal in its own right. The loss of the soft verge to the highway would also not be a reasonable ground for refusal in its own right, given the modest length and the existing absence of planting. However, this is also a negative aspect of the current proposal. This does not alter the conclusions outlined above.

Impact on Residential Amenity

- 7.26 Paragraphs 124 and 127 of the National Planning Policy Framework seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings. Policy KP2 of the Core Strategy seeks to secure improvements to the urban environment through quality design. Policy CP4 seeks to maintain and enhance the amenities, appeal and character of residential areas.

- 7.27 Policies DM1 and DM3 of the Development Management Document seek to support sustainable development which is appropriate in its setting, and that *“protects the amenity of the site, immediate neighbours, and surrounding area, having regard to matters including privacy, overlooking, outlook, noise and disturbance, sense of enclosure/overbearing relationship, pollution, daylight and sunlight”*.
- 7.28 The Design and Townscape Guide also states that *“the Borough Council is committed to good design and will seek to create attractive, high-quality living environments”* and that *“extensions must respect the amenity of neighbouring buildings and ensure not to adversely affect light, outlook or privacy of the habitable rooms in adjacent properties”*.
- 7.29 The part single-storey, part-two storey rear extension would project a further 1m rearward from the main part of the building, compared to the existing single storey rear projection. The separation distance to the rear boundary of the site with the side boundary of no.34 Leigh Gardens would be 12m. It is considered that this would be sufficient as to not create a significant actual or perceived loss of privacy over and above the existing situation.
- 7.30 The rear extension would be situated approximately 3.1m from the boundary with no.87 Western Road in common with the existing extension, and would be sited some 1.8m from the boundary with no.81 Western Road, compared to the existing 4.8m. It would be 1m longer than the existing extension and would have a relative rear projection of some 6.1m compared to the rear face of no.81. Rather than being a single storey it would as a result of the development be two storeys. The two storey element would be some 4.4m in depth.
- 7.31 Having no side windows, the rear extension would functionally eliminate the overlooking to either side currently resulting from the use of the roof as a terrace. This would provide a degree of benefit to neighbouring occupiers. The proposed side facing windows would be obscure glazed secondary windows. They would face on to the driveway of no.81 and towards its rear private amenity space. As they would be obscure glazed, which can be secured by a condition, there would not be a material loss of privacy arising to neighbouring occupiers.
- 7.32 Accounting for the 4.3m depth of the proposed two-storey side and rear element, dropping to single storey for its final 1.8m, and accounting for its height and its orientation and position relative to neighbouring dwellings and private amenity spaces at either side, it is considered that it would not lead to a materially harmful impact on occupiers in any relevant regard.
- 7.33 The proposed development would lead to a significant increase in the potential number of occupiers present in the semi-detached property at any one time. This degree of intensification would at times be discernible to the adjoining occupiers in particular, through general activity and comings and goings. This is a negative aspect of the proposal in the site context which is one characterised by single family dwellings and in which the existing use is two larger flats. However it would not justify a refusal of planning permission given the existing use and configuration of the building.

7.34 It is considered on balance that the proposed development is acceptable and policy-compliant with regard to neighbour amenities.

Living Conditions for Future Occupiers

7.35 At para.127 the NPPF states that planning decisions should ensure that developments create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users. Due regard may be had for the national Housing Technical Standards.

7.36 Policy DM1 of the Development Management Document requires that developments provide an internal and external layout that takes account of all potential users. Policy DM8 states that the internal environment of all new dwellings must be high quality and flexible to meet the changing needs of residents.

7.37 Further to this, from the 1st October 2015 the National Housing Standards have been adopted and state that the following internal floor space is required to ensure the development is in line with Building Control requirements:

- One bed, two person flat: 50sqm;
- Two bed, three person flat: 61sqm.

7.38 The proposed floor areas shown on the submitted plans for Flats 1 and 3 would exceed those required under the National Housing Standards. The slight shortfall with respect to Flats 2 and 4 would not be materially harmful in the context. Satisfactory outlook and daylight conditions would be available to all flats and the earlier concern in relation to the depth of rooms has been overcome by proposed secondary, obscure glazed windows.

7.39 Policy DM8 of the Development Management Document states that all new dwellings must make provision for usable private outdoor amenity space for the enjoyment of intended occupiers. The Council's Design and Townscape Guide states that "*Outdoor space significantly enhances the quality of life for residents and an attractive useable garden area is an essential element of any new residential development*".

7.40 Policy DM8 of the Development Management Document states that all new dwellings should meet the Lifetime Homes Standards, which from the 1st of October 2015 have been substituted by Building Regulation M4(2).

7.41 Part M4(2) of the Building Regulations adopted by the National Technical Housing Standards requires accessible and adaptable dwellings. Building regulations require 'reasonable provision' with reference to Part M4(2) on conversion schemes.

7.42 No details have been supplied to demonstrate that the development would be accessible and adaptable for all. By reason of being a conversion it is considered that it would be unfeasible to require the development to accord with the abovementioned standards. Therefore the application should not be refused on the grounds that it has not been demonstrated that the development would accord with Part M4(2) of the Building Regulations.

- 7.43 The proposal would form a 4 unit apartment building with 129sqm of useable outdoor amenity space. It is considered that this quantity and quality of outdoor amenity space would be adequate to meet the domestic and recreational needs of occupiers.
- 7.44 Refuse and cycle storage has been shown on the submitted plans and this could be required through a condition on any grant of planning permission. It would be situated adjacent a neighbouring outbuilding and, on this basis, would not be materially harmful to neighbour amenities. The proposal is on balance considered acceptable and compliant with the policies relating to living conditions.

Traffic and Transportation Issues

- 7.45 Policy DM15 of the Development Management Document requires that all development should meet the minimum off-street parking standards and as such, one parking space would be required for each proposed flat.
- 7.46 The submitted plans indicate that four off-street car parking spaces would be made available at the site. A cycle storage area has been shown on the submitted plan. Further details of secure cycle storage would be required and could be secured as a condition of any planning permission.
- 7.47 The Council's Vehicle Crossing Policy & Application Guidance is a material consideration. The proposal includes the creation of a vehicular crossover of a minimum 10.6m width with a 1m wide island. The vehicle crossovers would not exceed the maximum permitted width of 4.8m.
- 7.48 The highways officer has not objected to the application. The proposal is considered acceptable and policy compliant in regard to parking and highway safety.

Sustainability

- 7.49 Policy KP2 of the Core Strategy states that *"All development proposals should demonstrate how they will maximise the use of renewable and recycled energy, water and other resources. This applies during both construction and the subsequent operation of the development. At least 10% of the energy needs of new development should come from on-site renewable options (and/or decentralised renewable or low carbon energy sources), such as those set out in SPD 1 Design and Townscape Guide"*.
- 7.50 Policy DM2 of the Development Management Document part (iv) requires water efficient design measures that limit internal water consumption to 105 litres per person per day (lpd) (110 lpd when including external water consumption). Such measures will include the use of water efficient fittings, appliances and water recycling systems such as grey water and rainwater harvesting. Whilst details have not been submitted for consideration at this time, this could be dealt with by condition if the application were deemed acceptable.
- 7.51 A condition can be attached to any planning permission, requiring energy efficient design measures, water efficient design measures and permeable surfacing, for example. The proposals are considered acceptable with reference to these matters.

Community Infrastructure Levy (CIL)

- 7.52 This application is CIL liable. If the application had been recommended for approval, a CIL charge would have been payable. If an appeal is lodged and allowed the development will be CIL liable. Any revised application would also be CIL liable.

8 Conclusion

- 8.1 Having taken all material planning considerations into account, it is found that the proposed development would be unacceptable and contrary to the objectives of the relevant development plan policies and guidance. The proposed development of higher density flats would be out of keeping with the general character of the area as larger family dwellings. The amenities of neighbours would be harmed by the degree of general activity introduced by the high density of accommodation in this context. The proposal has overcome the earlier concerns in regard to design and highways matters. However, for the above reasons, the proposed development is unacceptable and fails to comply with planning policy. The benefits of the proposal, such as removing the roof terrace at the rear, and provision of additional dwellings, are not considered to outweigh the harm identified. The revised proposals have not overcome all the earlier reasons for refusal.

9 Recommendation

REFUSE PLANNING PERMISSION for the following reasons:

- 01. The proposed side and rear extension would, by reason of its scale, size, design and width, including the limited set-back from the front elevation of the main building, result in an overly dominant, disproportionate and unsympathetic addition, to the significant detriment of the character and appearance of the existing building and the wider street scene. This would be unacceptable and contrary to the National Planning Policy Framework; Policies KP2 and CP4 of the Core Strategy (2007); Policies DM1 and DM3 of the Development Management Document (2015) and advice contained within the adopted Design and Townscape Guide (2009).**

The Local Planning Authority has acted positively and proactively in determining this application by identifying matters of concern with the proposal and determining the application within a timely manner, clearly setting out the reason(s) for refusal, allowing the Applicant the opportunity to consider the harm caused and whether or not it can be remedied by a revision to the proposal. The detailed analysis is set out in a report prepared by officers. In the circumstances the proposal is not considered to be sustainable development. The Local Planning Authority is willing to discuss the best course of action.

10 Informatives

Please note that this application would be liable for a payment under the Community Infrastructure Levy Regulations 2010 (as amended) if planning permission had been granted. Therefore, if an appeal is lodged and subsequently allowed, the CIL liability will be applied. Any revised application would also be CIL liable.

85 WESTERN ROAD, LEIGH-ON-SEA.

RESIDENTIAL USE: 5NO. APARTMENTS

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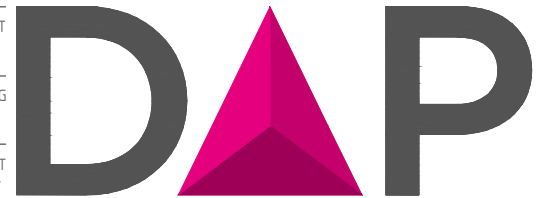


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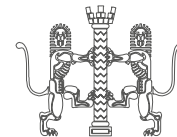
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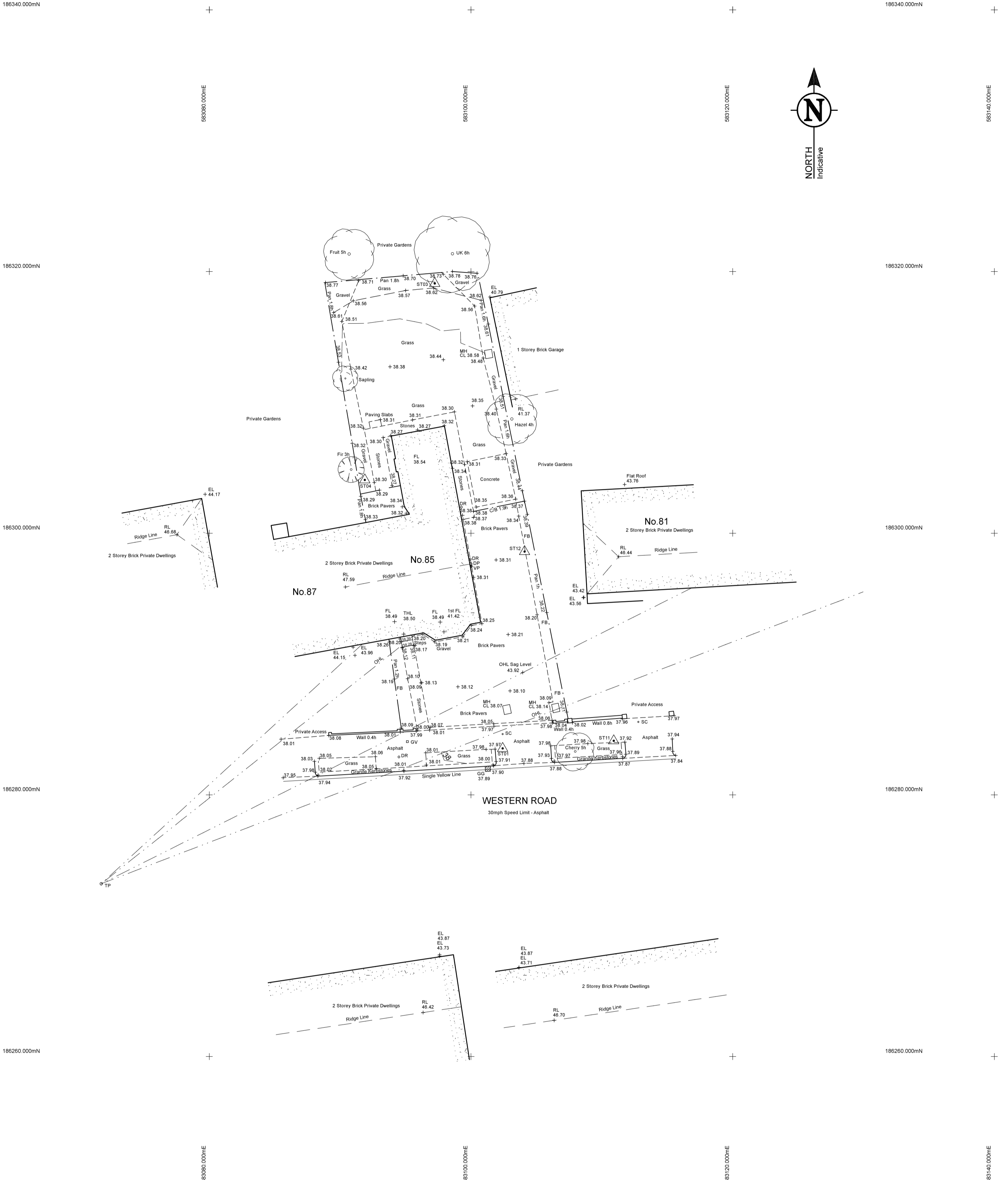
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TOPOGRAPHICAL & MEASURED BUILDING SURVEYS

ABBREVIATIONS & SYMBOLS

| | | |
|---------------------------|----------------------------|------------------------------|
| AH Ach Head Height | FH Fire Hydrant | RSJ Rolled Steel Joist |
| AR Assumed Route | FBD Floor Board Direction | SI Sign Post |
| AV Air Valve | FH Fire Hydrant | SP Arch Spring Point Height |
| BS Baffle Beacon | FL Floor Level | SV Stop Valve |
| BH Bore Hole | FP Flag Pole | SW Surface Water |
| BL Bed Level | FW Foul Water | SY Stay |
| BO Bolted | GG Gully Grate | Tac Tackle Pairing |
| B-P Brace Post | GV Gas Valve | TC Telecom Cover |
| BS Bus Stop | GH Head Height | TH Trial Pit |
| BU Bush | IC Inspection Cover | THL Threshold Level |
| BW Barbed Wire Fence | IL Invert Level | TL Traffic Light |
| BX Box (Utilities) | IR Iron Railings | ToW Top of Wall |
| CB Close Board Fence | KO Kerb Outlet | TP Telegraph Pole |
| CH Cill Height | LP Lamp Post | TV Cable TV Cover |
| CL Cover Level | MH Manhole | UB Universal Beam |
| CL Chain Link Fence | MP Marker Post | UC Unknown Cover |
| C-Lar Ceiling Level | NB Name Board | UK Unknown Tree |
| Col Column | OHL Overhead Line (approx) | USB Under Side Beam |
| C/P Chestnut Paling Fence | Pa Panel Fence | UTL Unable To Lift |
| CR Cable Rail | PB Post Box | VP Vent Pipe |
| DC Drainage Channel | PM Parking Meter | WS Waste Bin |
| DH Door Head Height | PO Post | WH Weep Hole |
| DP Down Pipe | PR Post & Rail Fence | WL Water Level |
| DR Drain | PWF Post & Wire Fence | WM Water Meter |
| EL Eaves Level | P/Wall Partition Wall | WO Wash Out |
| EP Electric Pole | RE Rodding Eye | WC Floor to Ceiling Height |
| ER Earth Road | RL Ridge Level | FC Floor to False Ceiling Ht |
| ET EP+Transformer | RP Reflector Post | RS Road Sign |
| FB Floor Bed | RS Road Sign | SC Survey Control Station |
| FBD Floor Board Direction | RSD Roller Shutter Door | |

DRAWING NOTE

Topographical Surveys

Trees are drawn to scale showing the average canopy spread. Descriptions and heights should be used as a guide only.

All building names, descriptions, number of storeys, construction type including roof line details are indicative only and taken externally from ground level.

All below ground details including drainage, voids and services have been identified from above ground and therefore all details relating to these features including: sizes, depth, description etc will be approximate only. All critical dimensions and connections should be checked and verified prior to starting work.

Detail, services and features may not have been surveyed if obstructed or not reasonably visible at the time of the survey.

Measured Building Surveys

Measurements to internal walls are taken to the wall finishes at approx 1m above the floor level and the wall assumed to be vertical.

Cill heights are measured as floor to the cill and head heights are measured from cill to the top of window.

General

The contractor must check and verify all site and building dimensions, levels, utilities and drainage details and connections prior to commencing work. Any errors or discrepancies must be notified to Survey Solutions immediately.

The accuracy of the digital data is the same as the plotting scale implies. All dimensions are in metres unless otherwise stated.

SURVEY CONTROL CO-ORDINATES

| STATIONS | EASTINGS | NORTHINGS | LEVEL | DESCRIPTION |
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| ST02 | 583157.658 | 186287.572 | 37.917 | PK Nail |
| ST03 | 583097.230 | 186319.093 | 38.705 | Peg/Nail |
| ST04 | 583091.884 | 186304.072 | 38.283 | Peg/Nail |
| ST11 | 583110.912 | 186284.136 | 37.973 | PK Nail |
| ST12 | 583104.097 | 186298.593 | 38.282 | PK Nail |

The survey control listed is only to be used for topographical surveys at the stated scale. All control must be checked and verified prior to use.

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Do not scale from this drawing.

SURVEY GRID AND LEVEL DATUM

The co-ordinate system established for this survey is related to Ordnance Survey (OS) national grid at a single point using GPS Smartnet, then orientated to Grid North with a scale factor of 1.000.

The level datum established for this survey is related to Ordnance Survey (OS) using GPS Smartnet.

To avoid discrepancies, any co-ordinated data used in conjunction with this survey must be derived directly from this control data.

The major contour interval is 1 metre, the minor contour interval is 0.500 metres.

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LAND SURVEYING BUILDING SURVEYING UNDERGROUND SURVEYING

PROJECT TITLE
85 WESTERN ROAD, LEIGH-ON-SEA, SOUTHEND-ON-SEA, ESSEX, SS9 2PT

DRAWING DETAIL
TOPOGRAPHICAL SURVEY
Sheet 1 of 1

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| CHECKED BY | HF | APPROVED BY | JM |
| DRAWING NUMBER | 21208 se-01 | REVISION | |
| | | ISSUE DATE | 26/01/2018 |

DWG STATUS: FINAL

ISSUE DATE: 26/01/2018

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85 WESTERN ROAD, LEIGH-ON-SEA

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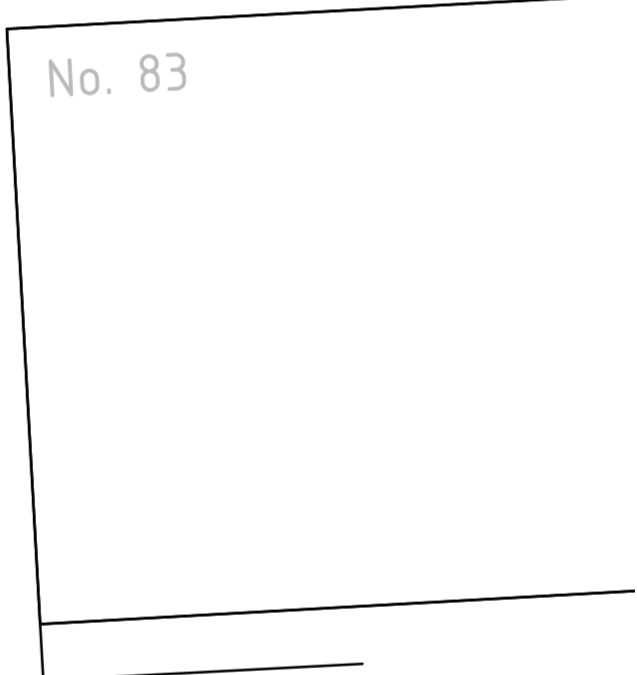
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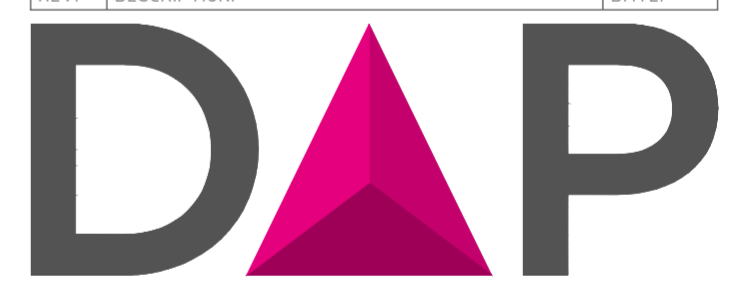
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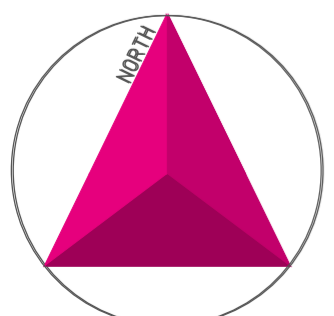
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ISSUE STATUS
RIBA STAGE 3 - PLANNING

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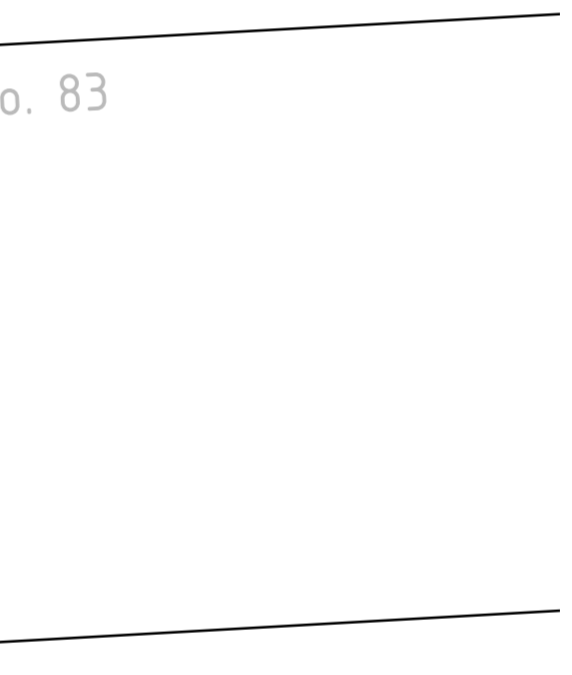
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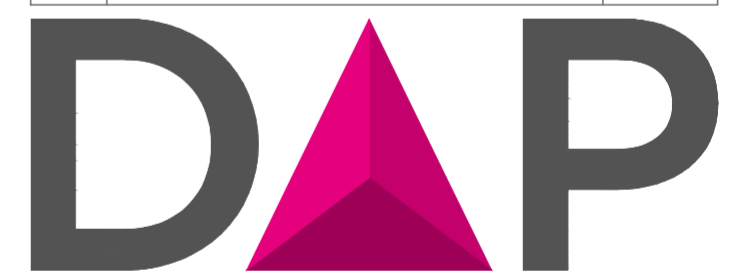
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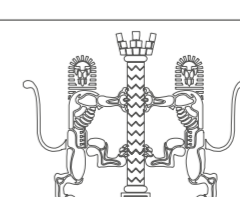
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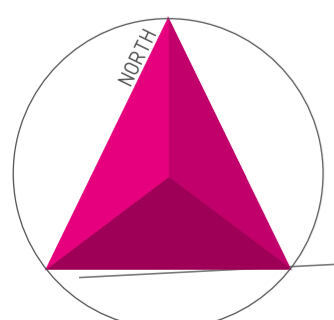
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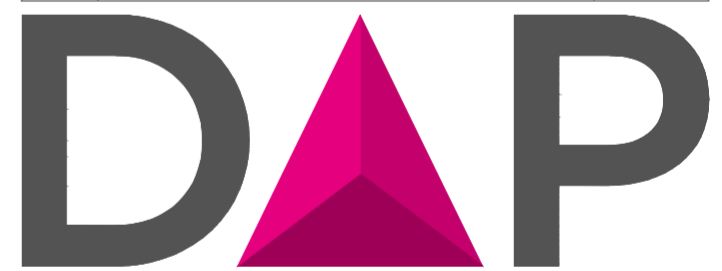
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VEHICULAR ACCESS PLAN

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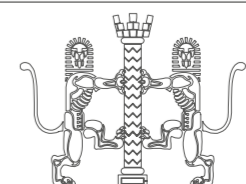
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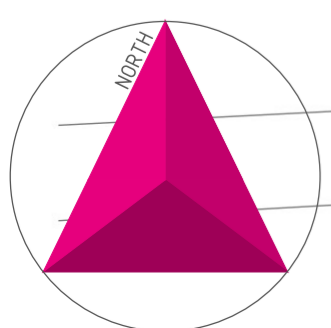
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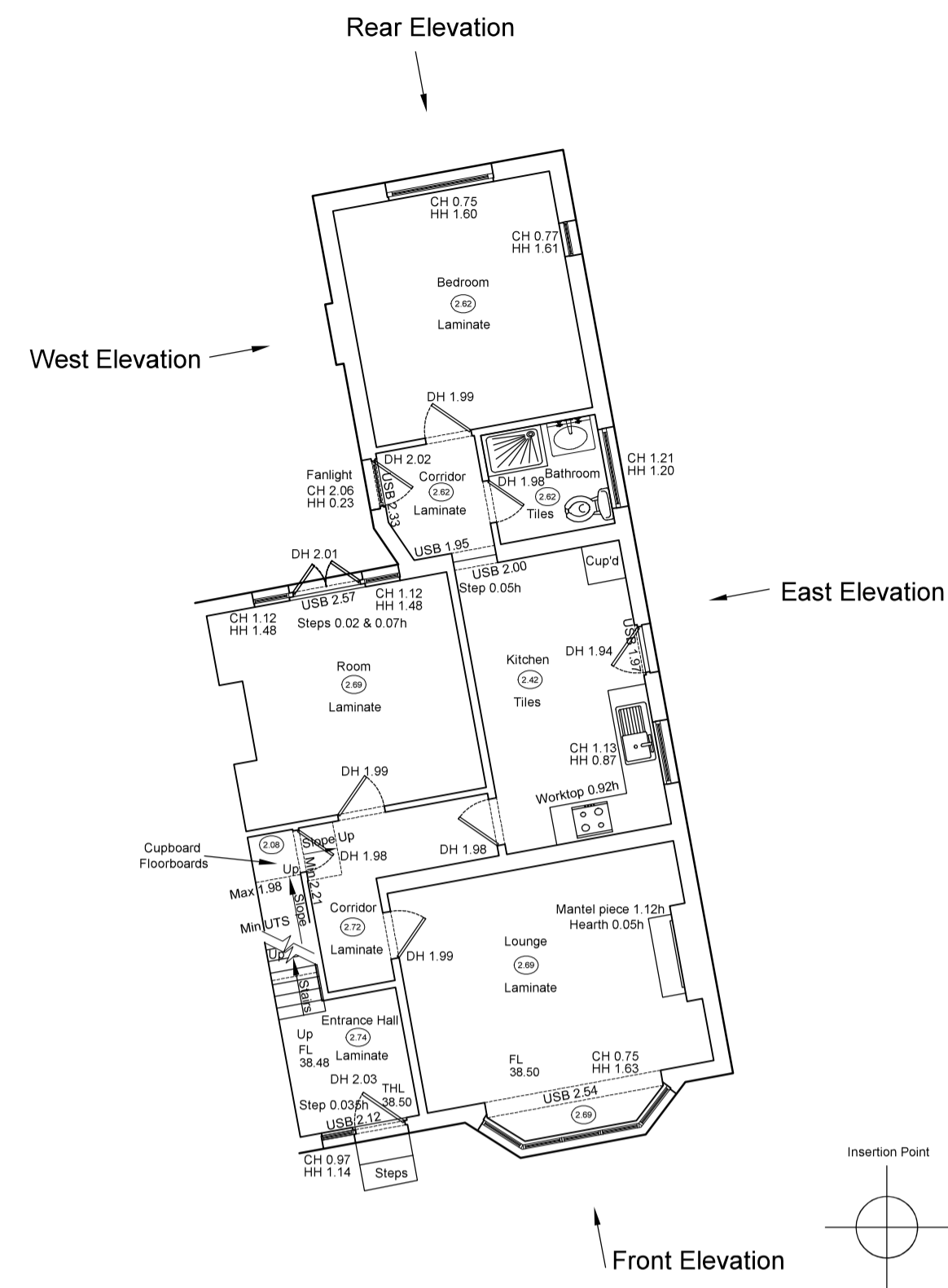
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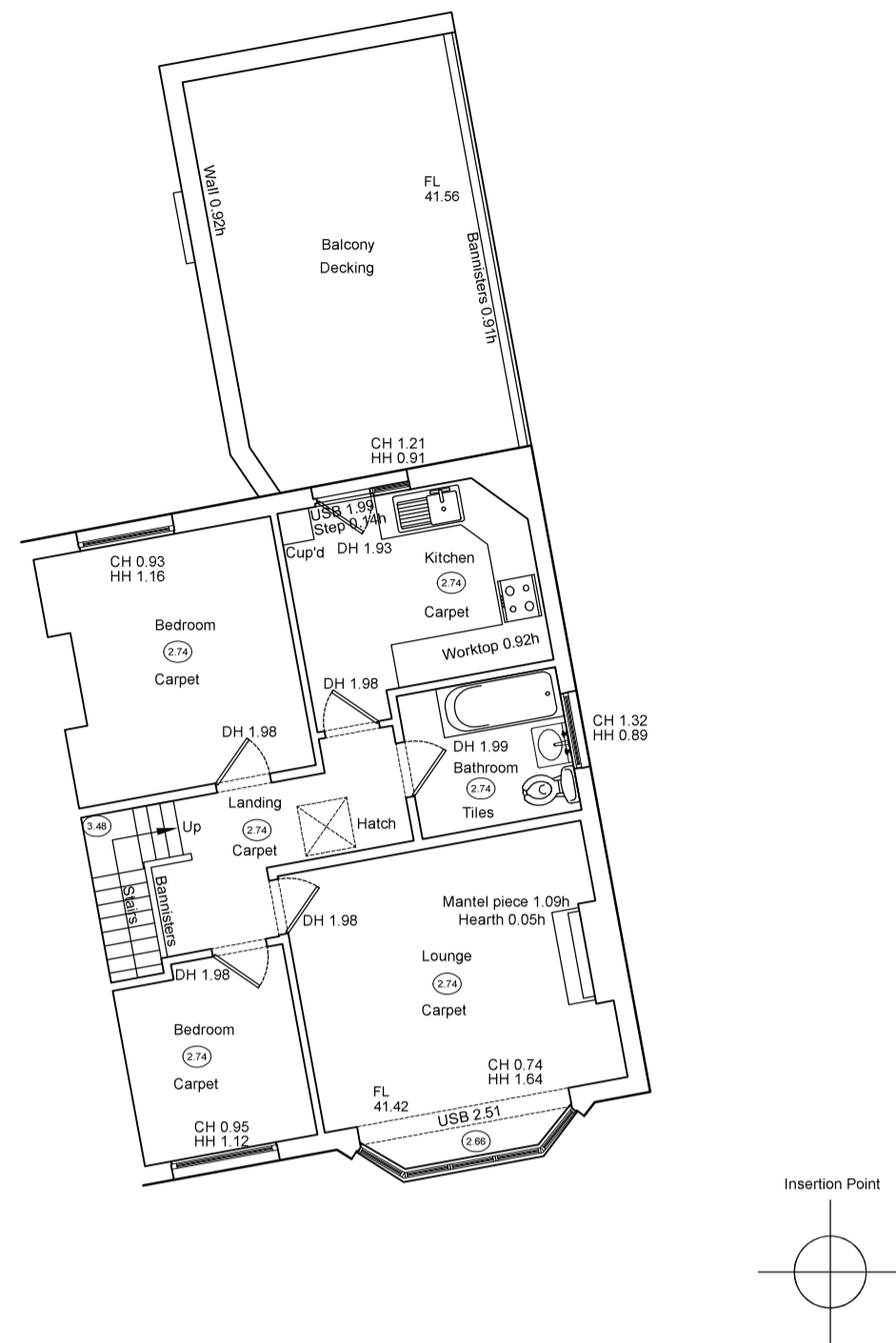
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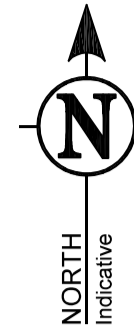
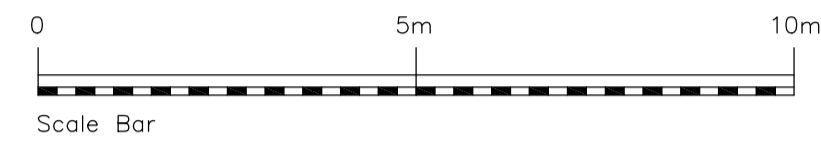
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GROUND FLOOR PLAN



FIRST FLOOR PLAN



TOPOGRAPHICAL & MEASURED BUILDING SURVEYS

ABBREVIATIONS & SYMBOLS

| | | |
|---------------------------|----------------------------|-----------------------------|
| AH Arch Head Height | FH Fire Hydrant | RSJ Rolled Steel Joist |
| AR Assumed Route | FBD Floor Board Direction | SI Sign Post |
| AV Air Valve | FL Floor Level | SP Arch Spring Point Height |
| BB Barbed Beason | FP Flag Pole | SV Stop Valve |
| BH Bore Hole | FW Four Water | SW Surface Water |
| BL Bed Level | GG Gully Grate | SY Stay |
| BQ Bolard | GV Gas Valve | Tac Tactile Paving |
| BRP Brace Post | HH Head Height | TC Telecom Cover |
| BS Bus Stop | IC Inspection Cover | TH Trial Pit |
| BU Bush | IL Invert Level | THL Threshold Level |
| BW Barbed Wire Fence | IR Iron Railings | TL Traffic Light |
| BX Box (Utilities) | KO Kerb Outlet | TP Top of Wall |
| CBS Close Board Fence | LP Lamp Post | TV Telegraph Pole |
| CH Cill Height | MH Manhole | UC Universal Beam |
| CL Cover Level | MP Marker Post | UB Unknown Cover |
| CL Chain Link Fence | NE Name Board | UK Unknown Tree |
| C Lev Ceiling Level | OHL Overhead Line (approx) | USB Under Side Beam |
| Col Column | Pan Panel Fence | U/L Unable To Lift |
| C/P Chestnut Paving Fence | PM Parking Meter | VP Vent Pipe |
| CR Castle Rise | PO Post | WB Waste Bin |
| DC Drainage Channel | POB Post Box | WH Weep Hole |
| DH Door Head Height | PPR Post & Rail Fence | WL Water Level |
| DP Down Pipe | P/W Post & Wire Fence | WM Water Meter |
| DR Drain | P/Wall Partition Wall | WO Wash Out |
| EL Eaves Level | RE Rooding Eye | WC Floor to Ceiling Height |
| EP Electric Pole | RL Rig/L Level | FC Floor to Ceiling Hit |
| ER Earth Road | RP Reflector Post | RS Road Sign |
| ET EP-Transformer | RS Road Sign | RSD Roller Shutter Door |
| FB Flower Bed | RSD Roller Shutter Door | SCS Survey Control Station |
| FBD Floor Board Direction | | |

DRAWING NOTE

Topographical Surveys

Trees are drawn to scale showing the average canopy spread. Descriptions and heights should be used as a guide only.

All building names, descriptions, number of storeys, construction type including roof line details are indicative only and taken externally from ground level.

All below ground details including drainage, voids and services have been identified from above ground and therefore all details relating to these features including, sizes, depth, description etc will be approximate only. All critical dimensions and connections should be checked and verified prior to starting work.

Detail, services and features may not have been surveyed if obstructed or not reasonably visible at the time of the survey.

Measured Building Surveys

Measurements to internal walls are taken to the wall finishes at approx 1m above the floor level and the wall assumed to be vertical.

Cill heights are measured as floor to the cill and head heights are measured from cill to the top of window.

General

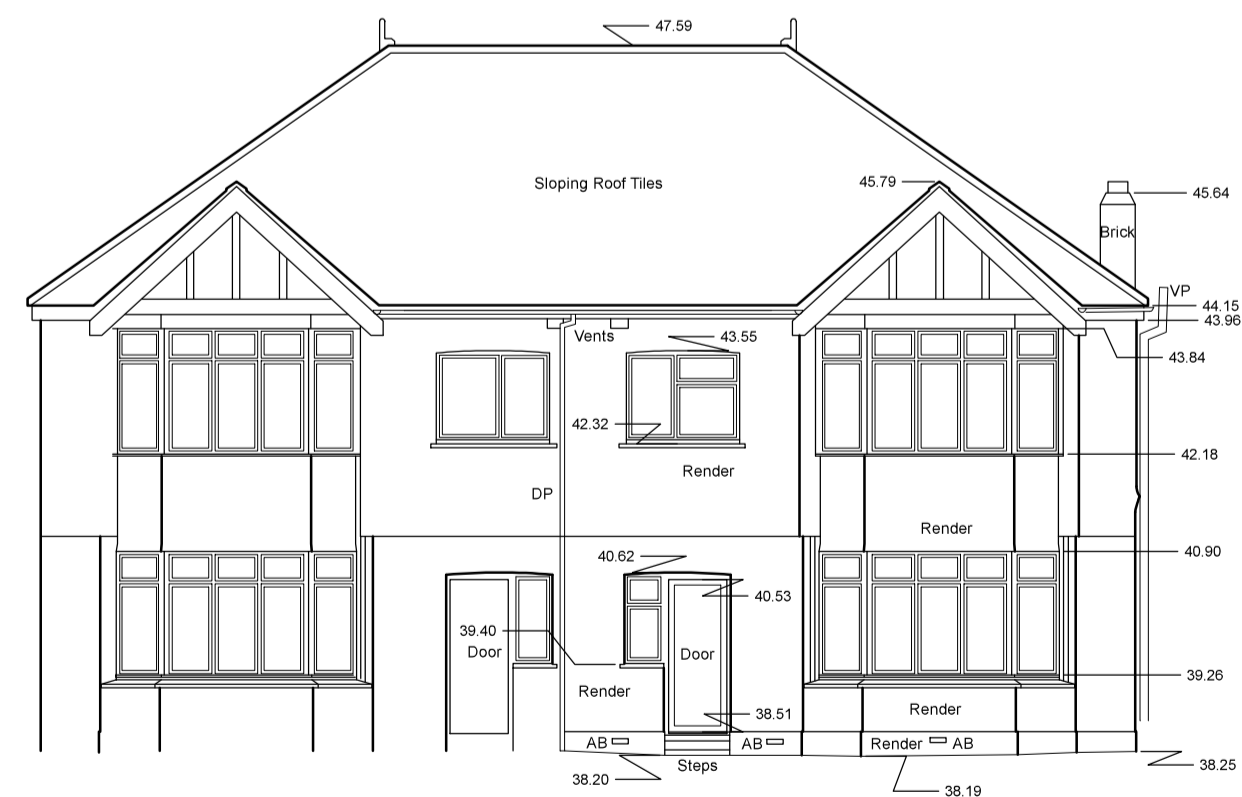
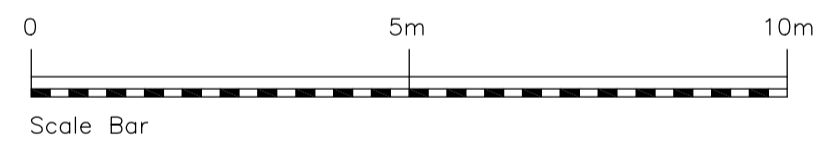
The contractor must check and verify all site and building dimensions, levels, utilities and drainage details and connections prior to commencing work. Any errors or discrepancies must be notified to Survey Solutions immediately.

The accuracy of the digital data is the same as the plotting scale implies. All dimensions are in metres unless otherwise stated.

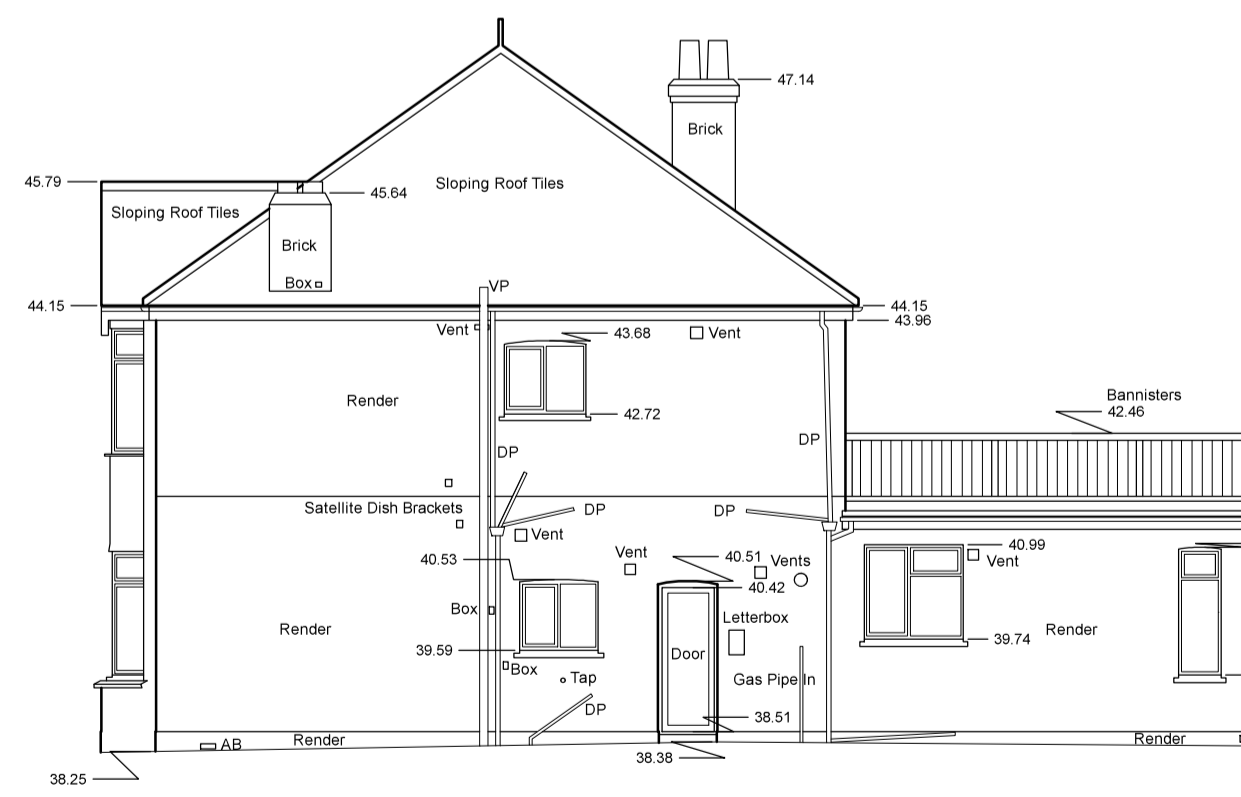
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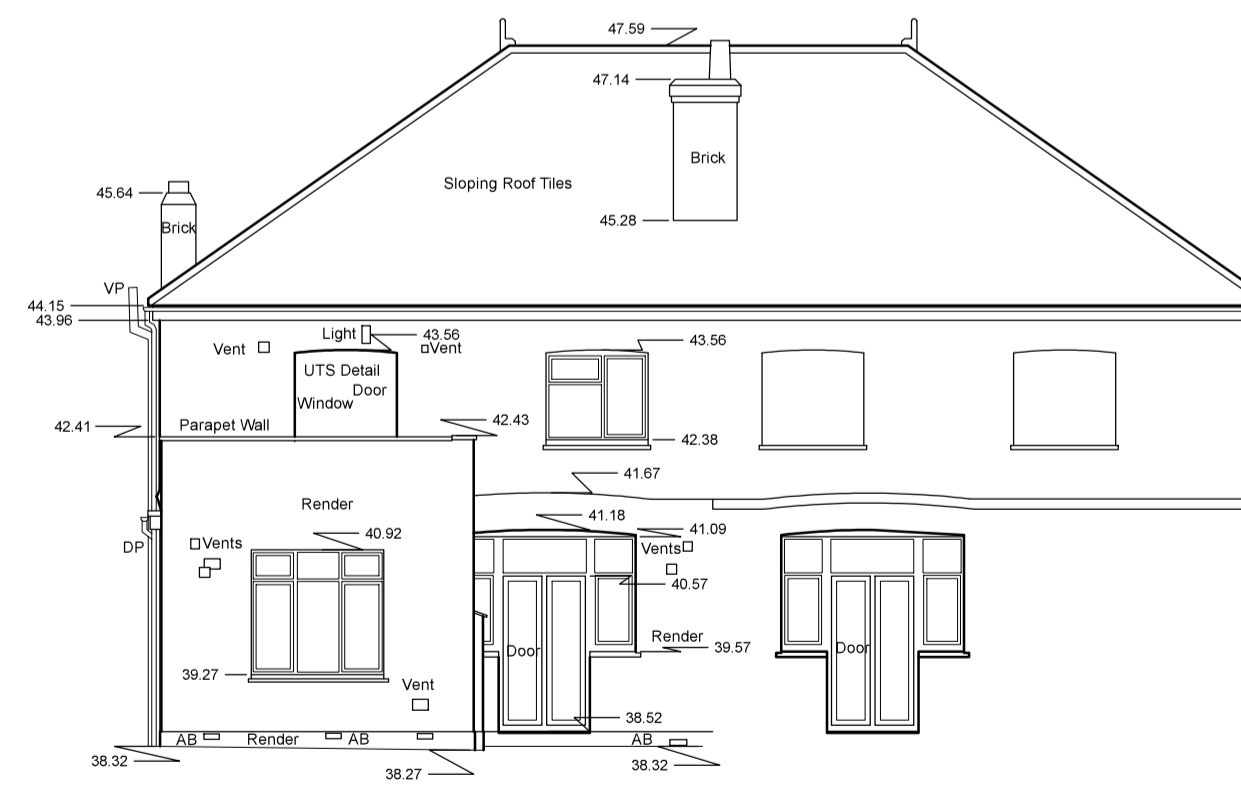
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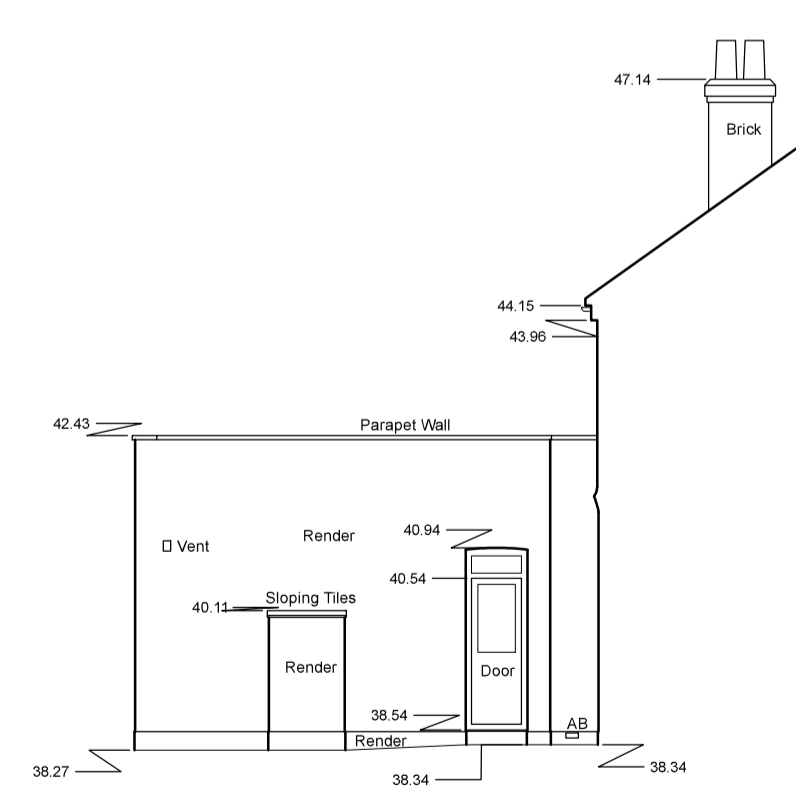
FRONT ELEVATION



EAST ELEVATION



REAR ELEVATION



WEST ELEVATION

35.00 metres above Ordnance Datum

SURVEY CONTROL CO-ORDINATES

| STATIONS | EASTINGS | NORTHINGS | LEVEL | DESCRIPTION |
|----------|----------|-----------|-------|-------------|
|----------|----------|-----------|-------|-------------|

SURVEY GRID AND LEVEL DATUM

All levels relate to and are taken from drawing 211 se-01.

| REV | DESCRIPTION | DRAWN | APPR | DATE |
|-----|-------------|-------|------|------|
|-----|-------------|-------|------|------|



Ipswich Coventry Yeovil Norwich Perth Nottingham Brentwood
 Tel No: 0845 0405 969 Fax No: 0845 0405 970
 www.survey-solutions.co.uk enquiries@survey-solutions.co.uk
 LAND SURVEYING BUILDING SURVEYING UNDERGROUND SURVEYING

PROJECT TITLE
 85 WESTERN ROAD, LEIGH-ON-SEA,
 SOUTHEND-ON-SEA, ESSEX, SS9 2PT

DRAWING DETAIL
 MEASURED BUILDING SURVEY
 FLOOR PLANS & ELEVATIONS Sheet 1 of 1

| | |
|-------------------------------|--------------------------------------|
| CLIENT Mr. JAY NEALE | SCALE 1:100 |
| SURVEYOR ADK | SURVEY DATE 28/01/2018 |
| CHECKED BY HF | APPROVED BY JM |
| DRAWING NUMBER 21208 se-02 | REVISION ISSUE DATE 23/01/2018 |

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85 WESTERN ROAD, LEIGH-ON-SEA.

RESIDENTIAL SCHEME: 5NO. APARTMENTS

SCALE BAR 1:100



FRONT ELEVATION



SIDE ELEVATION



REAR ELEVATION



SIDE ELEVATION

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68

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PROJECT
RESIDENTIAL DEVELOPMENT

DRAWING
AS EXISTING ELEVATIONS

CLIENT
SEARCH FLOW

ADDRESS
85 WESTERN ROAD
LEIGH-ON-SEA
SOUTHEND-ON-SEA
SS9 2PT

DATE 14.05.18 SCALE 1:100 @ A2 DRAWN BY LD CHECKED BY JB

PROJECT No. DWG No. REVISION

905.003.00

ISSUE STATUS
EXISTING

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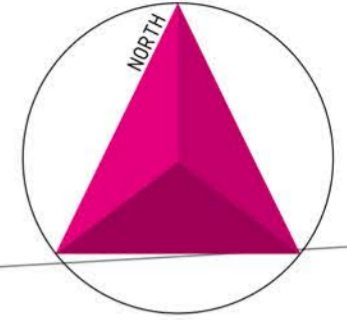
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WESTERN ROAD

No. 83

No. 87

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PROJECT
RESIDENTIAL DEVELOPMENT

DRAWING
AS PROPOSED
SITE LAYOUT + GROUND FLOOR PLAN

CLIENT
SEARCH FLOW

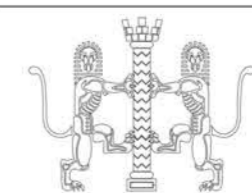
ADDRESS
85 WESTERN ROAD
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ESSEX
SS9 2PT

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905. 210. 00

ISSUE STATUS
RIBA STAGE 3 - PLANNING



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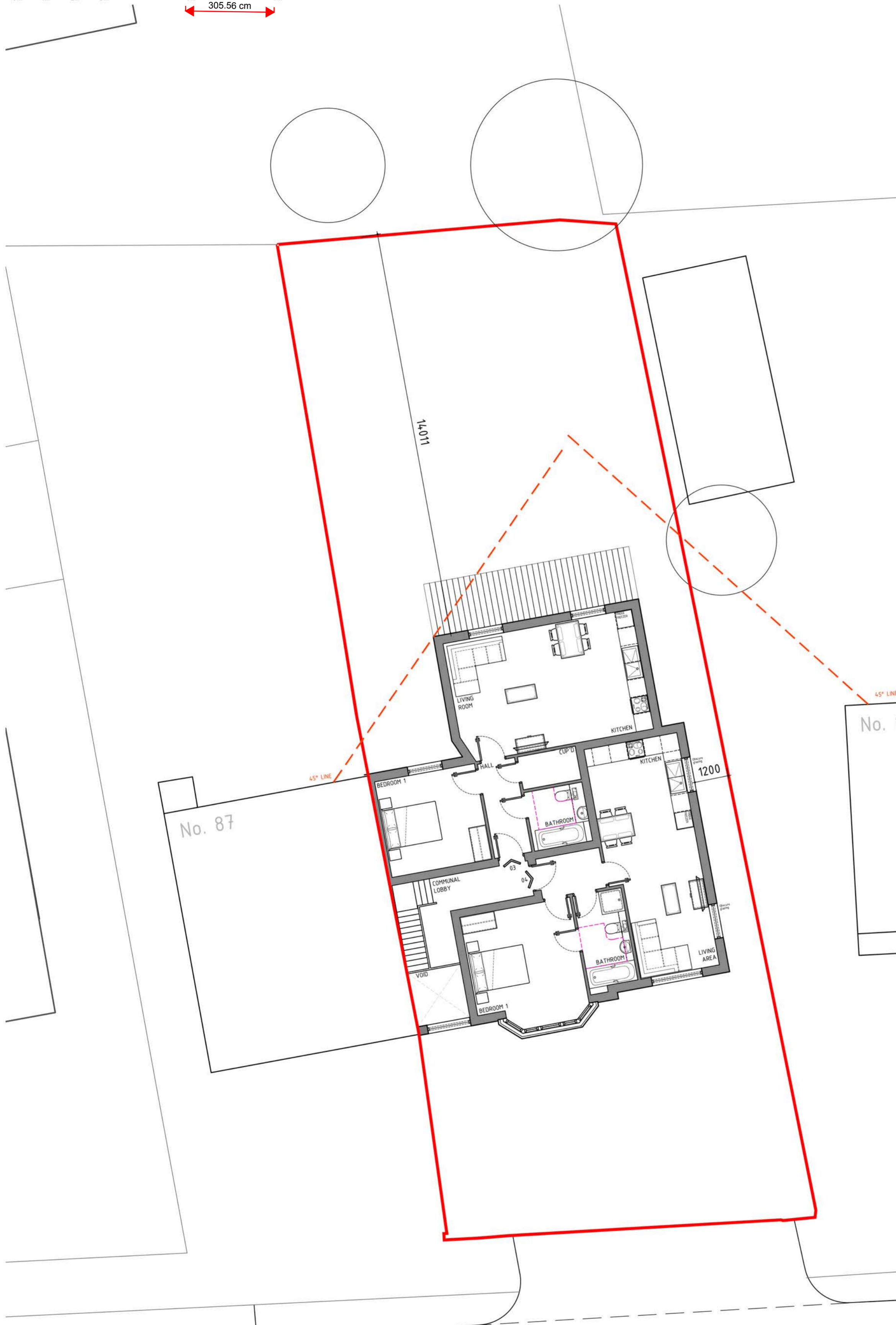
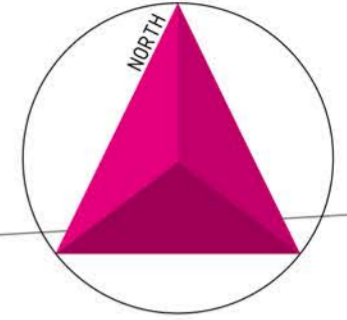
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DRAWING
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SITE LAYOUT + FIRST FLOOR PLAN

CLIENT
SEARCH FLOW

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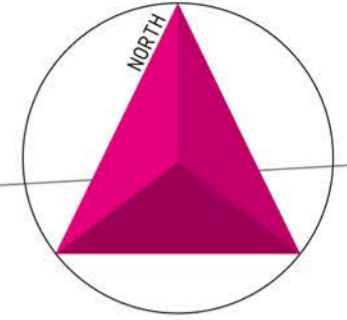
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DRAWING
AS PROPOSED
SITE LAYOUT + ROOF PLAN

CLIENT
SEARCH FLOW

ADDRESS
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LEIGH-ON-SEA
ESSEX
SS9 2PT

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905. 212. 00

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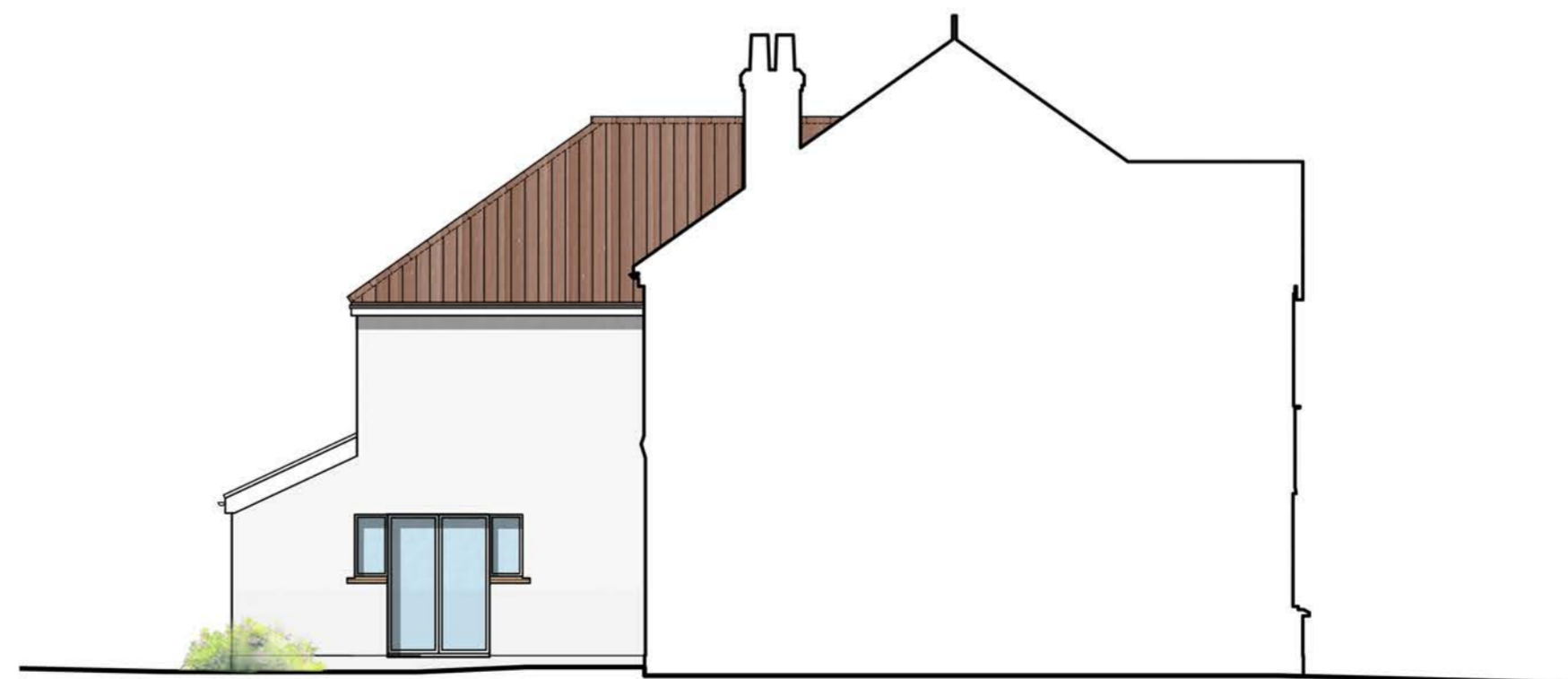
AS PROPOSED FRONT ELEVATION





AS PROPOSED SIDE ELEVATION



AS PROPOSED REAR ELEVATION



AS PROPOSED SIDE ELEVATION

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| DRAWING | | | |
| AS PROPOSED ELEVATIONS | | | |
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| SEARCH FLOW | | | |
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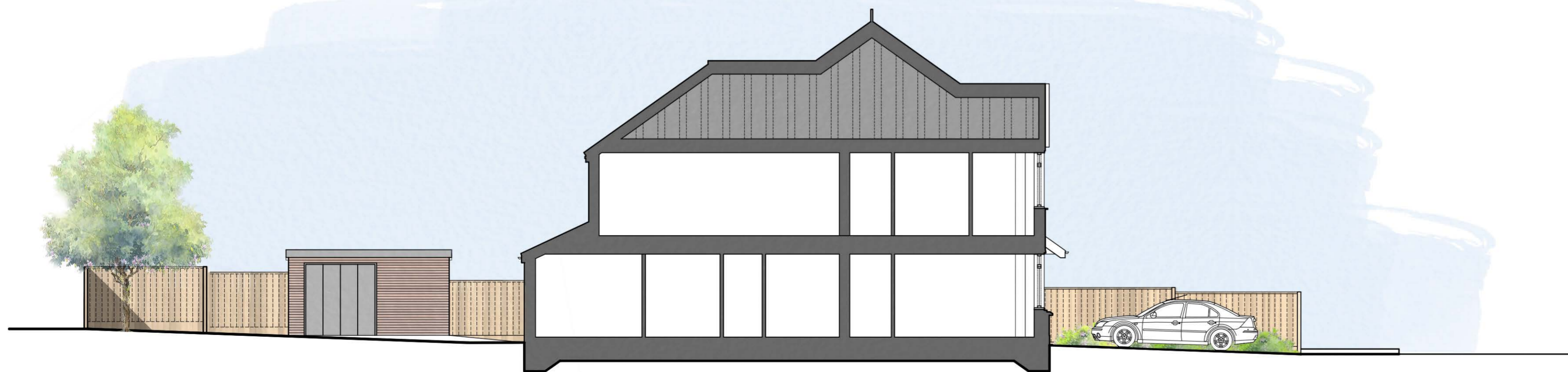
Residential Property:
89 Western Road, Leigh-on-Sea

Residential Property:
87 Western Road, Leigh-on-Sea

Residential Property:
85 Western Road, Leigh-on-Sea

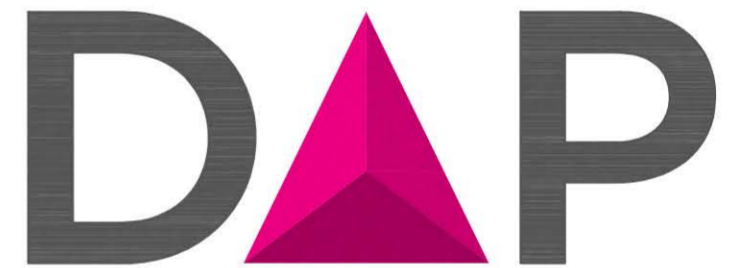
Residential Property:
83 Western Road, Leigh-on-Sea

AS PROPOSED STREET SCENE ALONG WESTERN ROAD



AS PROPOSED SITE SECTION

| REV. | DESCRIPTION | DATE |
|------|-------------|------|
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PROJECT
RESIDENTIAL DEVELOPMENT

DRAWING
AS PROPOSED
STREET SCENE & SITE SECTION

CLIENT
SEARCH FLOW

ADDRESS
85 WESTERN ROAD
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RIBA STAGE 3: PLANNING

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85 WESTERN ROAD, LEIGH-ON-SEA

RESIDENTIAL DEVELOPMENT: 4NO. APARTMENTS

SCALE BAR 1:100



FRONT ELEVATION



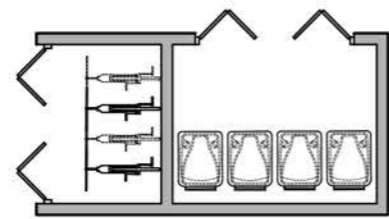
SIDE ELEVATION



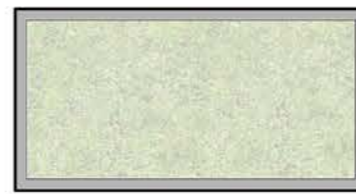
REAR ELEVATION



SIDE ELEVATION



GROUND FLOOR PLAN



ROOF PLAN

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TIMBER LARCH CLADDING



GREEN ROOF



METAL BI-FOLD DOORS

101

AS PROPOSED BIN AND BIKE STORE

| | | | | | | | |
|--------------|--|---|----------|--------------------------|-------------|---|----------|
| PROJECT | | RESIDENTIAL DEVELOPMENT | | | | | |
| DRAWING | | AS PROPOSED BIKE AND BIN STORE | | | | | |
| CLIENT | | SEARCH FLOW | | | | | |
| ADDRESS | | 85 WESTERN ROAD LEIGH-ON-SEA ESSEX SS9 2PT | | | | | |
| DATE | | SCALE | DRAWN BY | CHECKED BY | PROJECT No. | DWG No. | REVISION |
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| ISSUE STATUS | | RIBA STAGE 3 - PLANNING | | | | <p>a. 3 + 5 Hospital Approach The Millars Chelmsford ESSEX, CM1 7FA e. studio@daparchitecture.co.uk t. (0844) 854 9007 w. www.daparchitecture.co.uk</p> | |
| | | | | <p>905.215.00</p> | | <p>CHELMSFORD LONDON BIRMINGHAM</p> | |

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Site



81 to east

104



501105



87 to west

106



From no.81





81 side elevation

110



Looking north on site towards garden of no.34 Leigh Gardens



112

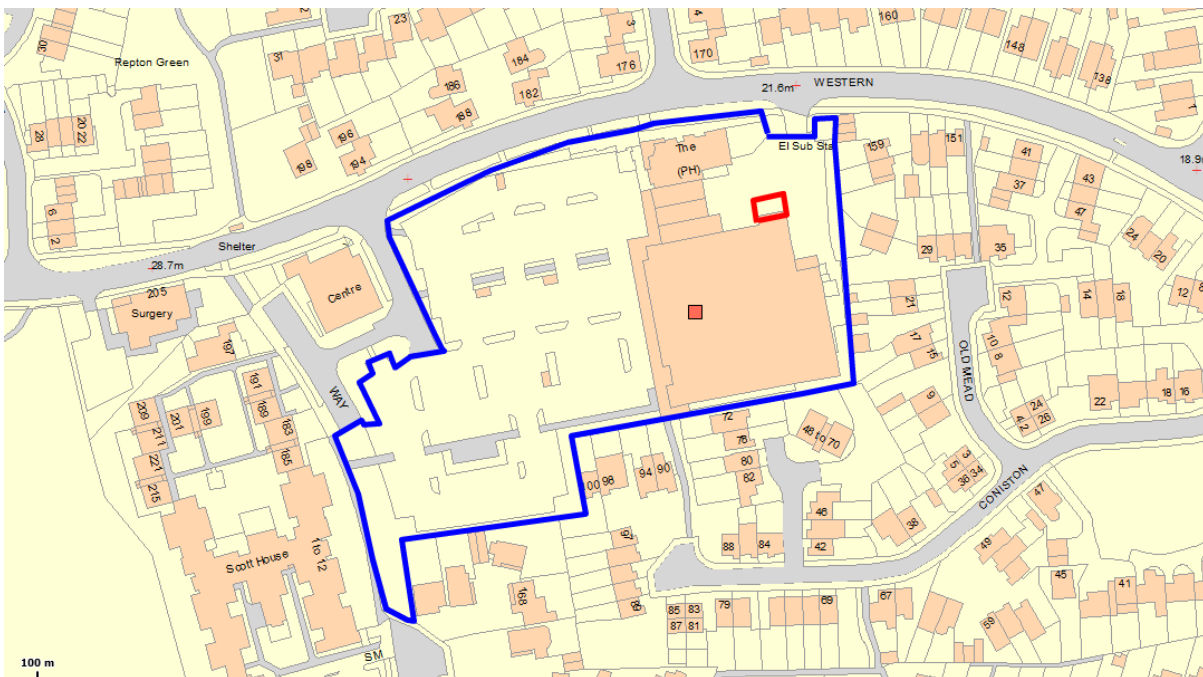




113

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| | | |
|-----------------------------|---|----------|
| Reference: | 20/00580/FUL | 7 |
| Ward: | St Laurence | |
| Proposal: | Install external refrigeration plant to North elevation | |
| Address: | Morrisons, Western Approaches, Eastwood, Essex, SS2 6SH | |
| Applicant: | WM Morrisons | |
| Agent: | Darton B3 | |
| Consultation Expiry: | 28.04.2020 | |
| Expiry Date: | 08.06.2020 | |
| Case Officer: | Scott Davison | |
| Plan Nos: | 20016, 20017, 90001B, 90002, 90003 & 90004 Noise Impact Noise Impact Assessment by Acoustic Control Ltd Reference: B5253 2020-05-04 R.& Plant Specification in the email from Darton B3 dated 1 st May 2020 | |
| Recommendation: | GRANT PLANNING PERMISSION subject to conditions | |



1 Site and Surroundings

- 1.1 The application site is a single storey building on the southern side of Western Approaches and also accessed from this road. There is a car park to the west of the store and the service area for the unit is to the rear (north) of the building and accessed directly from Western Approaches. There are various items of plant located externally on the roof of the store and in the service yard. There are a number of commercial units adjacent to the Morrison's supermarket including a public house. The areas surrounding the application site are residential with houses to the east in Old Mead and along Western Approaches.
- 1.2 The application property is not listed and is not located within a conservation area or within a Frontage Of Townscape Merit or within a Town Centre Shopping Frontage. It is not subject to any specific planning designations.

2 The Proposal

- 2.1 Planning permission is sought to install external refrigeration plant to north elevation of the building within the rear service yard. The proposal is comprised of the following:

Installation of 1 No. fridge pack & 1 No. gas cooler

Erection of refrigeration plant compound comprised of new palisade fencing and gates with new timber acoustic cladding fixed and Armco barriers

- 2.2 The proposal would result in the removal of an existing rear projection to the north elevation of the building and the removal of existing plant within it would be removed. The applicant has explained that new regulations require that by 2025, all refrigerators should be run on CO2 rather than a carbon emissive gas (which is currently the case). A container sited adjacent to the rear used for the storage of fireworks would also be removed.

3 Relevant Planning History

- 3.1 Install one internally illuminated fascia sign to front elevation- Granted (18/00284/ADV).
- 3.2 Change of use from betting offices (sui generis) to café (Class C3) - Prior Approval Granted (17/00231/PA3COU).
- 3.3 Install two internally illuminated fascia signs and one internally illuminated totem sign- Granted (17/00252/ADV).
- 3.4 Use Betting shop (Class Sui Generis) as Cafe (Class A3) (Lawful Development Certificate-Proposed) - Refused Certificate of Lawfulness (16/02266/CLP).
- 3.5 Use vacant retail unit (Class A1) as betting office (Class A2)- Permitted (04/00326/FUL)

4 Representation Summary

Public Consultation

- 4.1 Councillor Flewitt has called the application in for consideration by the Development Control Committee.
- 4.2 A site notice was displayed and 14 neighbours were notified in writing. No letters of representation have been received.

Environmental Health

- 4.3 No objections subject to conditions relating to noise.

5 Planning Policy Summary

- 5.1 National Planning Policy Framework (NPPF) (2019)
- 5.2 Core Strategy (2007) Policies KP1 (Spatial Strategy) KP2 (Development Principles), CP3 (Transport and Accessibility) and CP4 (Environment & Urban Renaissance)
- 5.3 Development Management Document (2015): Policy DM1 (Design Quality), Policy DM3 (Efficient and Effective Use of Land) & DM15 (Sustainable Transport Management)
- 5.4 The Design & Townscape Guide, 2009.
- 5.5 CIL Charging Schedule 2015

6 Planning Considerations

- 6.1 The main considerations are in relation to the principle of development, design and impact on the character of the area, traffic and transportation and the impact on residential amenity.

7 Appraisal

Principle of Development

- 7.1 The principle of the development is assessed against the NPPF, Core Strategy policies KP1, KP2 and CP4; Development Management Document Policies DM1, DM3 and the Design and Townscape Guide. Given that the proposed development will replace existing units, it accords in principle with all the aforementioned policies, thus no objection is raised subject to the material planning considerations detailed below.

Design, Impact on the Character of the area

- 7.2 The proposed external refrigeration plant will be located to the north elevation of the building and contained within an external compound which would project some 6.5m from the rear of the building and would be some 9.2m wide and 3m high. The new plant and equipment would be contained within the compound and would not be any higher than the fencing. An existing flat roof brick built rear projection some 8.2m wide, 5.2m high and 2.8m deep would be removed as a result of the development as would a shipping container (3m wide x 2.5m x 6m) which is used for the storage of fireworks.
- 7.3 The compound would have a larger footprint than the existing rear projection and container but would not be set any closer to the northern or eastern boundaries of the site than structures to be removed. In terms of impact on the character and appearance of the streetscene, the container and rear projection on the northern elevation of the building are visible in limited views from Western Approaches from the entrance to the service yard. The compound would be visible from dwellings to the east of the site in Oldmead. Although the proposed compound would have greater site coverage than the existing facilities it would not be set any closer to the eastern boundary of the site and sufficiently removed from the northern boundary of the site. It is considered that proposed development would be seen in the context of the service yard and would not result in any material harm to the character and appearance of the building or the wider streetscene.
- 7.4 In summary it is considered that the proposed development would not harm the visual amenities, character or appearance of the surrounding area by virtue of scale, form or appearance and is acceptable and compliant with development plan policy in the above regards.

Traffic and Transport Issues

- 7.5 The site is located in a rear service area which contains existing containers and associated equipment used by the Supermarket in their daily operations. Given that the proposed development would be located in a similar location to the facilities to be removed and is away from Western Approaches, it is not considered that it would result in any adverse impact on the functioning of the highway or pedestrian pavement. The proposal is therefore acceptable and policy compliant in the above regards.

Impact on neighbouring amenity

- 7.6 Policy KP2 of the Core Strategy seeks to secure improvements to the urban environment through quality design. Policy CP4 seeks to maintain and enhance the amenities, appeal and character of residential areas.
- 7.7 Policies DM1 and DM3 of the Development Management Document seek to support sustainable development which is appropriate in its setting. Policy DM1 requires new development to, *“Protect the amenity of the site, immediate neighbours, and surrounding area, having regard to privacy, overlooking, outlook, noise and disturbance, visual enclosure, pollution, and daylight and sunlight.”*

- 7.8 The Design and Townscape Guide also states that “the Borough Council is committed to good design and will seek to create attractive, high-quality living environments”.
- 7.9 The proposed development is for the installation of new refrigeration / air-conditioning plant and a surrounding compound. The proposed development would be located on the rear (north) elevation and an existing materially higher projection would be removed. The nearest residential properties are some 30m to the east of the site and the proposed development would not be any closer towards the eastern boundary of the site. It is not considered that the proposed development would give rise to any detrimental physical impact on nearby premises in terms of an undue sense of enclosure or a loss of outlook.
- 7.10 The Environmental Health (EH) consultation response notes that
- “there is already existing plant at the site and we do not have any records of complaints from the nearby dwellings” .and “the proposed plant appears to be more moderns [sic] and the plans show an acoustic fencing surround” . However EH consider that the development could lead to heightened aware of equipment from nearby occupiers and although no objections are raised, due to the limits of the information submitted in support of the application further information is required with regard to noise generation and conditions are requested to limit noise from the plant and assess it when installed.
- 7.11 Subject to conditions, the proposed development is considered acceptable and policy compliant in the above regards.

Community Infrastructure Levy CIL Charging Schedule 2015

- 7.12 The proposed development would not create any additional floorspace at the application site and therefore is not CIL liable.

8 Conclusion

- 8.1 Having taken all material planning considerations into account, it is found that subject to compliance with the attached conditions, the proposed development would be acceptable and compliant with the objectives of the relevant development plan policies and guidance. The principle of the development is found to be acceptable and the proposal would not cause harm to the character or appearance of the application building or the wider surrounding area. It is considered that the development would not cause material harm to the amenities of neighbouring residents or the safety of the highway. Therefore, it is recommended that planning permission is granted.

9 Recommendation

9.1 Members are recommended to: GRANT PLANNING PERMISSION subject to the following conditions:

01 The development hereby permitted shall begin not later than three years from the date of this decision.

Reason: Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

02 The development hereby permitted shall be carried out solely in accordance with the following approved plans: 20016, 20017, 90001B, 90002, 90003 & 90004 and the Plant Specification set out in the email from Darton B3 dated 1st May 2020

Reason: To ensure that the development is carried out in accordance with the provisions of the Development Plan.

03 Noise from plant and equipment including extract ventilation shall be limited to 10 dB(A) below the background noise level measured and expressed as a LA90, 15minutes from the boundary of the nearest residential property. Background noise levels shall be established for the following periods:

- Daytime 0700 to 1900**
- Evening 1900 to 2300**
- Night 2300 to 0700**

In order to establish background noise level a representative survey shall be undertaken in accordance with BS 4142:2014+A1:2019 at the boundary of the nearest residential properties. To demonstrate compliance prior to operation a post completion noise survey must be been undertaken by a suitably qualified acoustic consultant, and a report submitted to and approved in writing by the Local Planning Authority. r

Reason: In order to protect the amenities of occupiers of the development and surrounding residents in accordance with policies KP2 and CP4 of the Core Strategy (2007) and Policies DM1 and DM3 of the Development Management Document (2015).

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development, as set out within the National Planning Policy Framework. The detailed analysis is set out in a report on the application prepared by officers.

Informatives

01. You are advised that as the proposed works to your property creates no new floorspace the development benefits from a Minor Development Exemption under the Community Infrastructure Levy Regulations 2010 (as amended) and as such no charge is payable. See www.southend.gov.uk/cil for further details about CIL.

02. You should be aware that in cases where damage occurs during construction works to the highway in implementing this permission that Council may seek to recover the cost of repairing public highways and footpaths from any party responsible for damaging them. This includes damage carried out when implementing a planning permission or other works to buildings or land. Please take care when carrying out works on or near the public highways and footpaths in the borough.

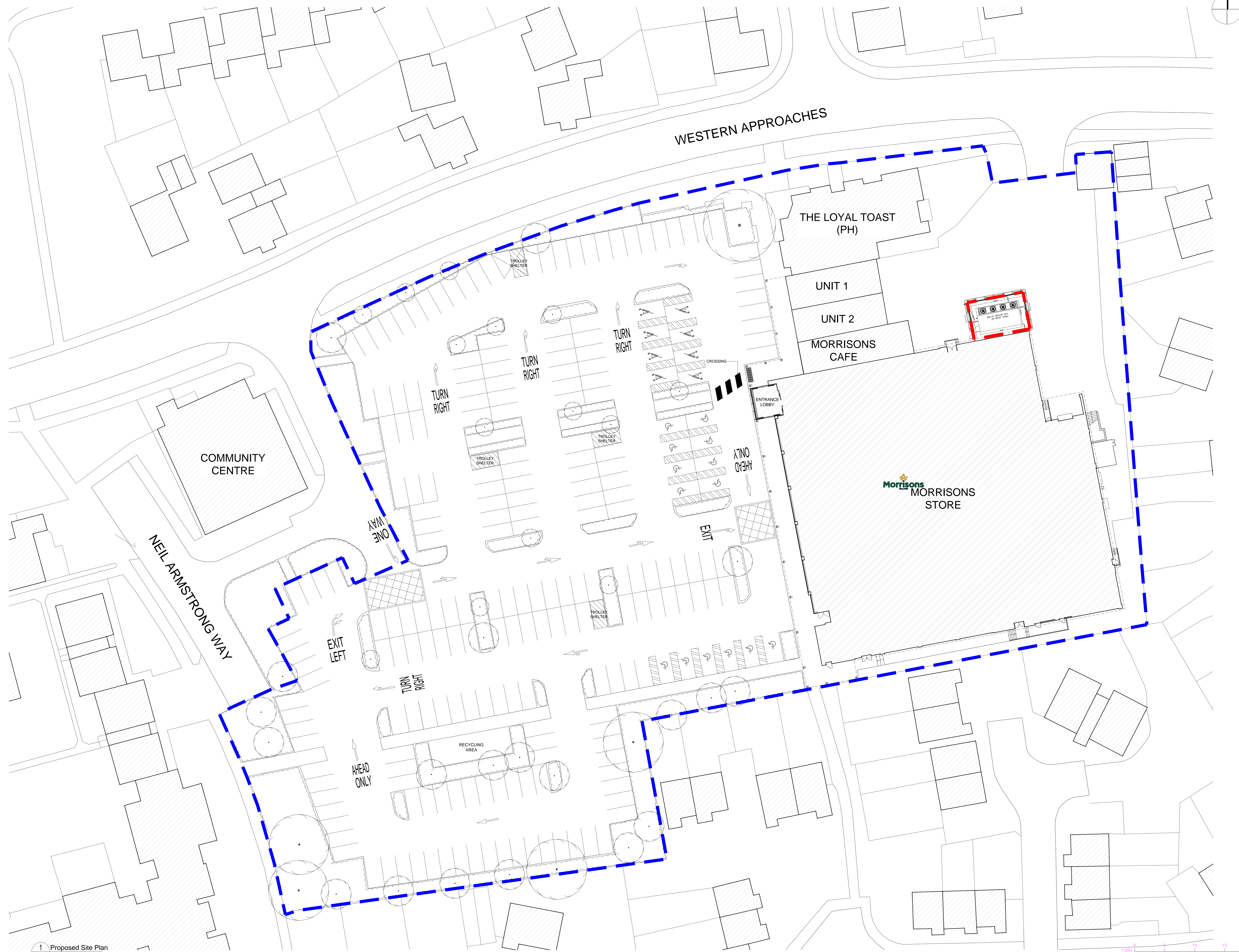
03 The applicant is reminded that this permission does not bestow compliance with other regulatory frameworks. In particular your attention is drawn to the statutory nuisance provisions within the Environmental Protection Act 1990 (as amended) and construction noise provisions within the Control of Pollution Act 1974. Applicants should contact the Council's Environmental Health Officer for more advice on 01702 215005 or at Regulatory Services, P.O. Box 5558, Southend-on-Sea Borough Council, Civic Centre, Victoria Avenue, Southend-on-Sea, SS2 6ZQ.

04 If construction works are to be considered outside of normal hours especially overnight it is recommended that the applicant applies for a prior consent application under section 61 of the Control of Pollution Act 1974. A consent enables the applicant to conduct the works without the worry of enforcement provided they comply with it. The applicant will have to submit details of any noisy works including type of plant and machinery to be used, proposed daily start and finish times, consultation with nearby residents and businesses and duration and time scales of the works. The applicant should contact the Regulatory Services Team at Southend-on-Sea Borough Council for details.

05 The applicant is also reminded that this permission is separate to the need to comply with Food Safety and Health & Safety at Work laws. These will include the Food Safety Act 1990 (as amended), the Food Hygiene (England) Regulations 2006, Regulation (EC) 852/2004 and the Health and Safety at Work Etc. Act 1974. Applicants should contact the Council's Regulatory Services Officer for Food and Health and Safety for more advice on 01702 215005 or at Regulatory Services, Southend-on-Sea Borough Council, Civic Centre, Victoria Avenue, Southend SS2 6ER.

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--- WMS Site Boundary Line
 - - - Proposed Plant Enclosure Application Boundary

| | |
|---------------------------|-------------------------------|
| 1:200 (Scale to Planning) | 1:100 (Scale to Construction) |
| NO. DATE | DESCRIPTION |

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 T: 0113 277 81 00 www.damtonb3.com

CLIENT
Morrisons

PROJECT
 Refrigeration Replacement 2020
 #201 WMS Southfield Eastwood

DRAWING
 General Arrangement
 Proposed Site Plan

| | | |
|------------|-------|---------|
| SCALE @ A0 | DRAWN | CHECKED |
| 1:200 | LA | PH |

CREATION DATE
 March 2020

REVISION

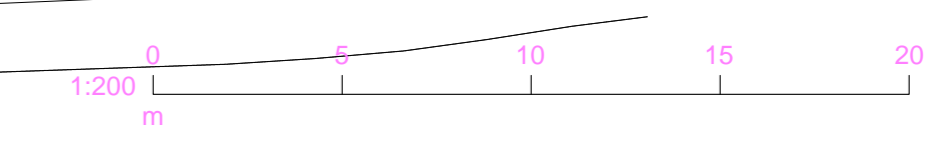
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PROJECT NO. COMPANY. ZONE. LEVEL. TYPE. ROLE. NUMBER

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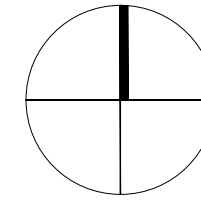
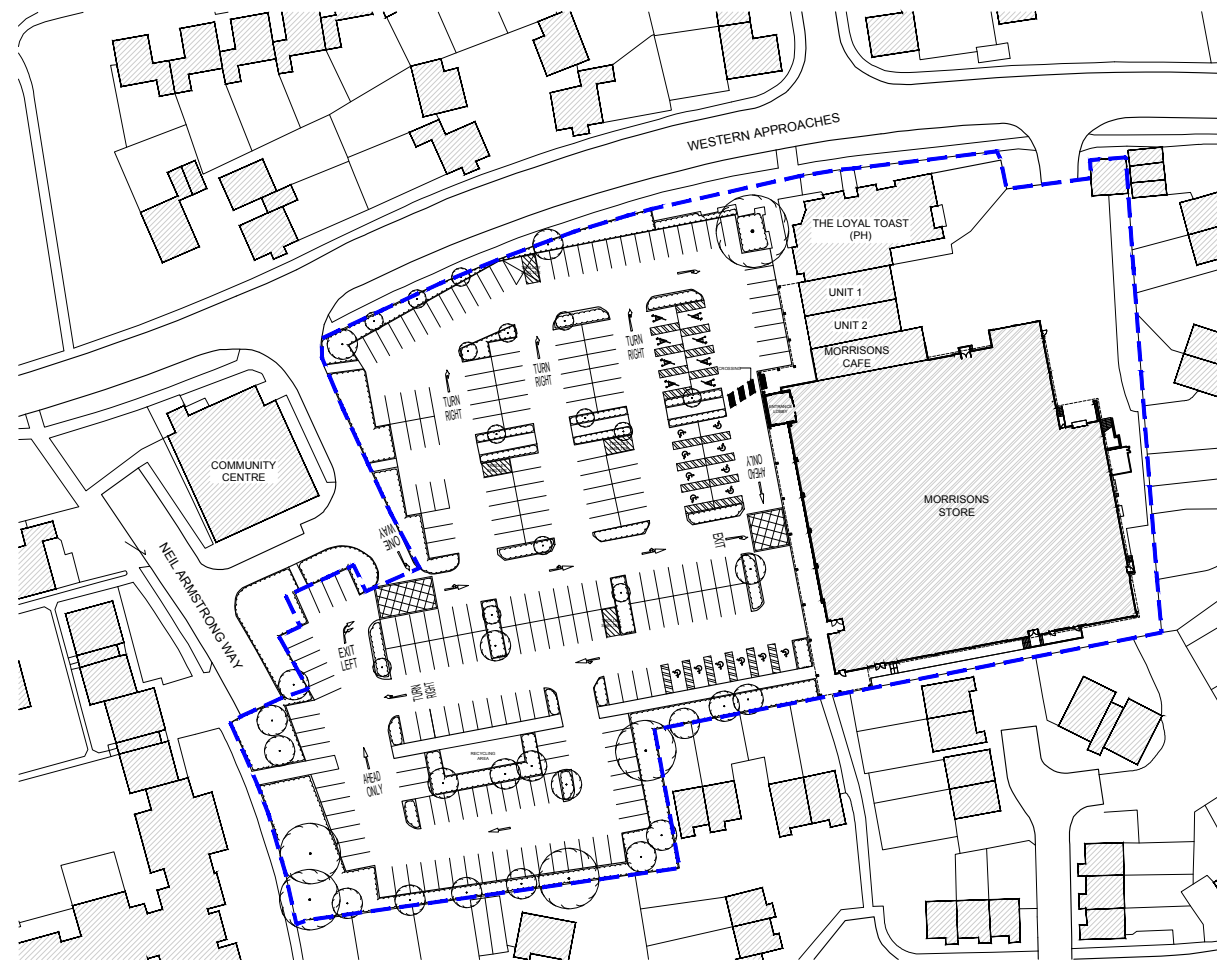
1 Proposed Site Plan
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521

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--- Site Boundary line

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| REV/DATE | DESCRIPTION | DR | CH |

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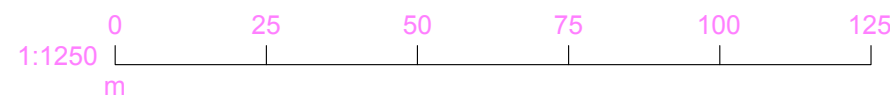
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| CREATION DATE MARCH 2020 | REVISION | |

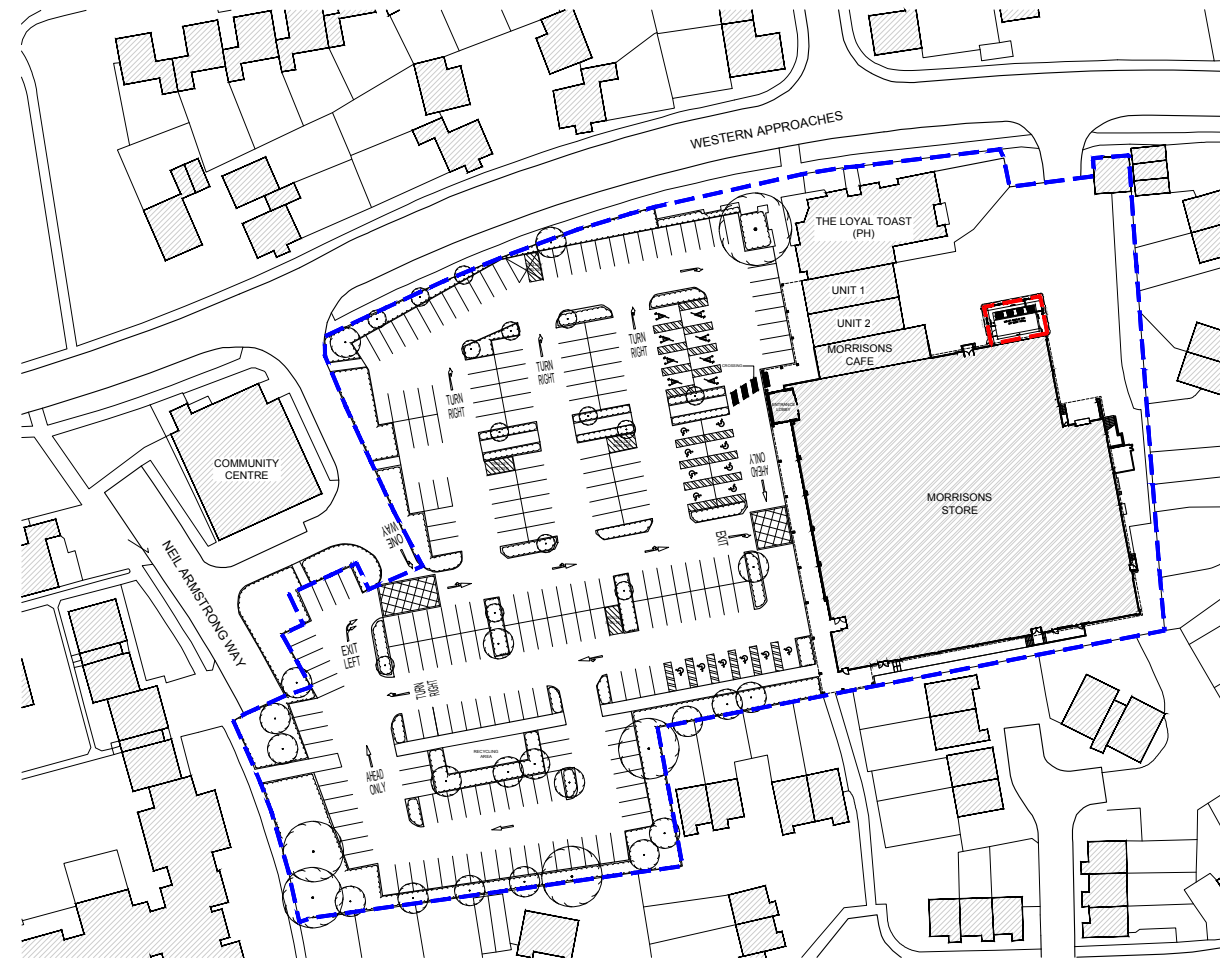
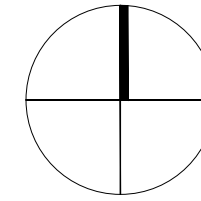
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1 Existing Site Location Plan
1:1250 @ A3



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--- Site Boundary line

--- Proposed Plant Enclosure Application Boundary

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| REV DATE | DESCRIPTION | DR | CH |

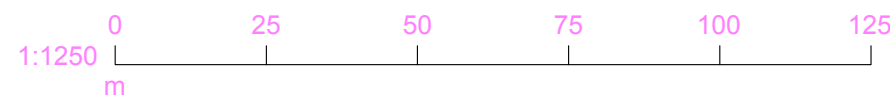
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1 Proposed Site Location Plan
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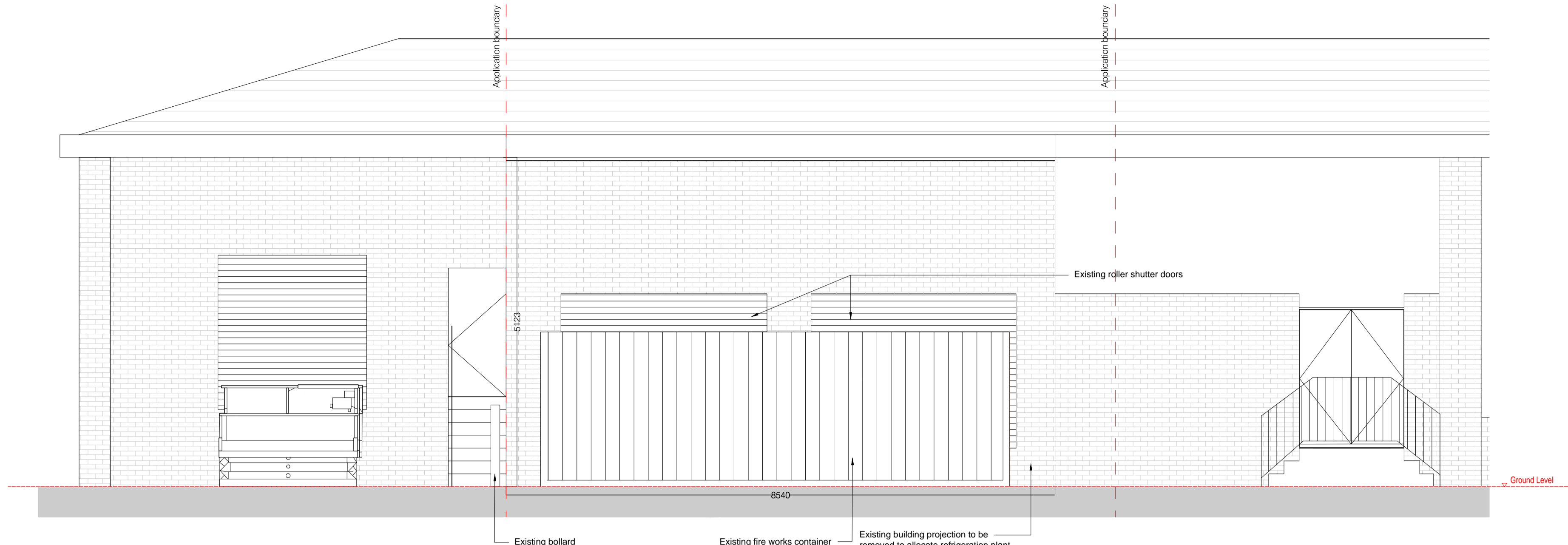
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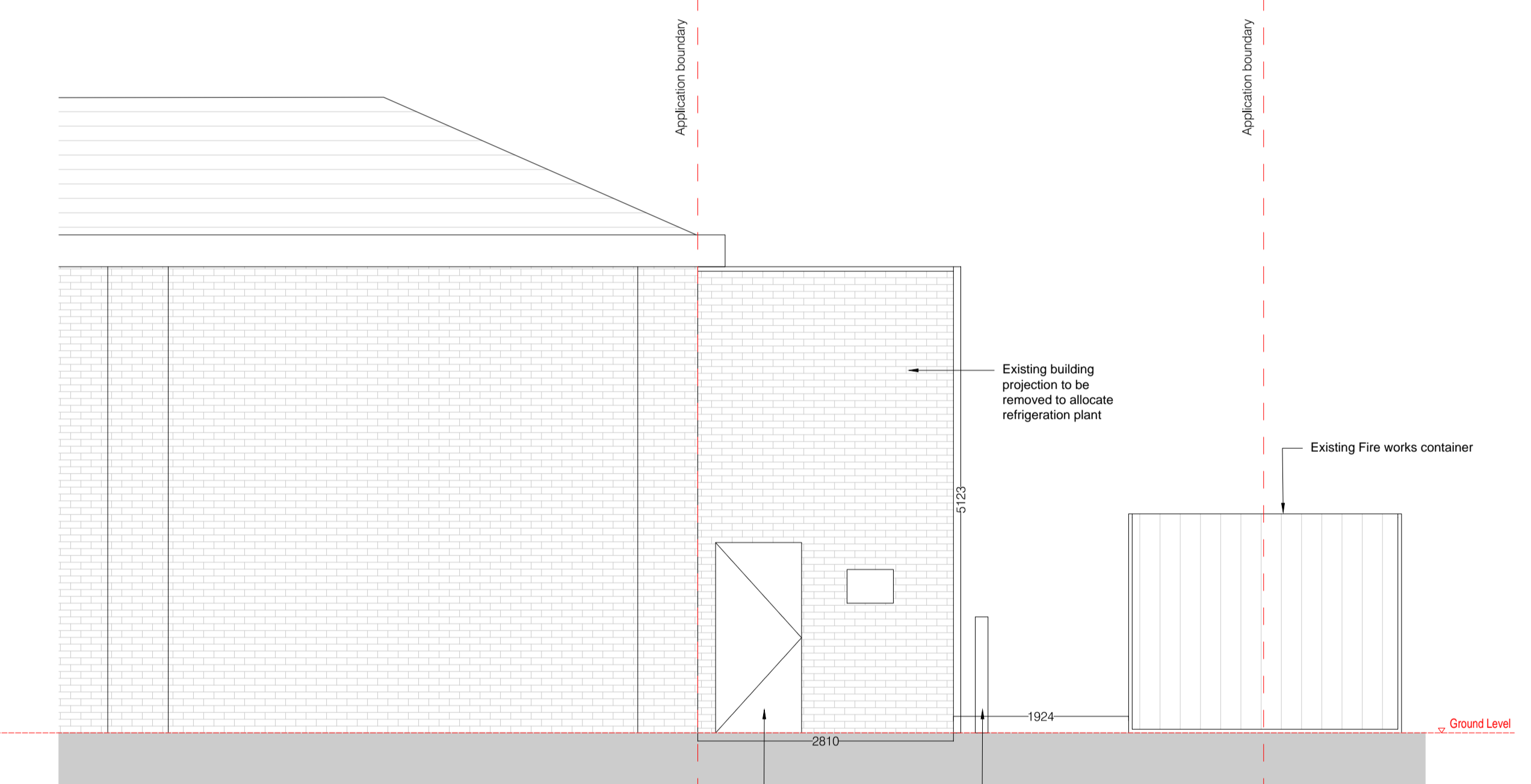
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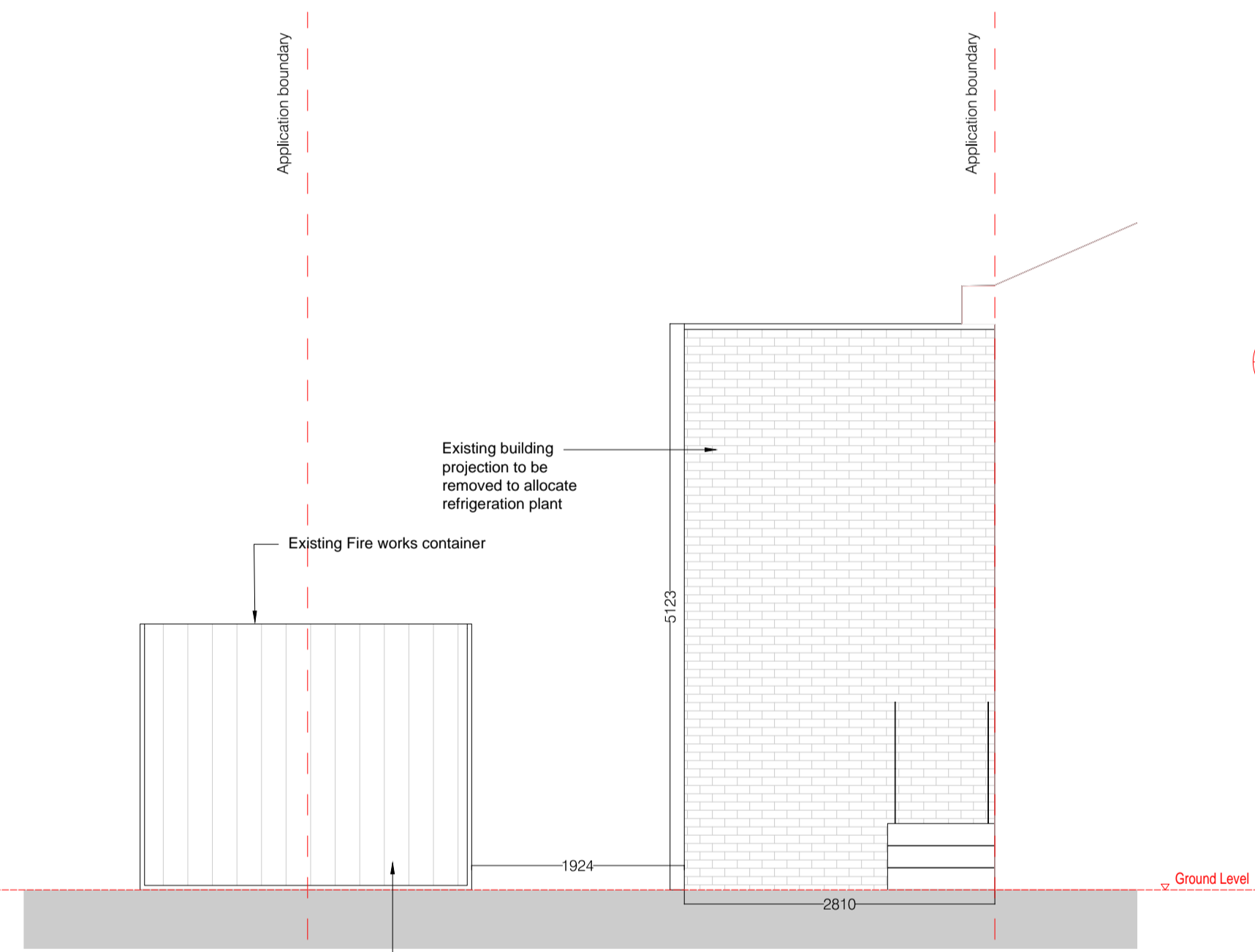
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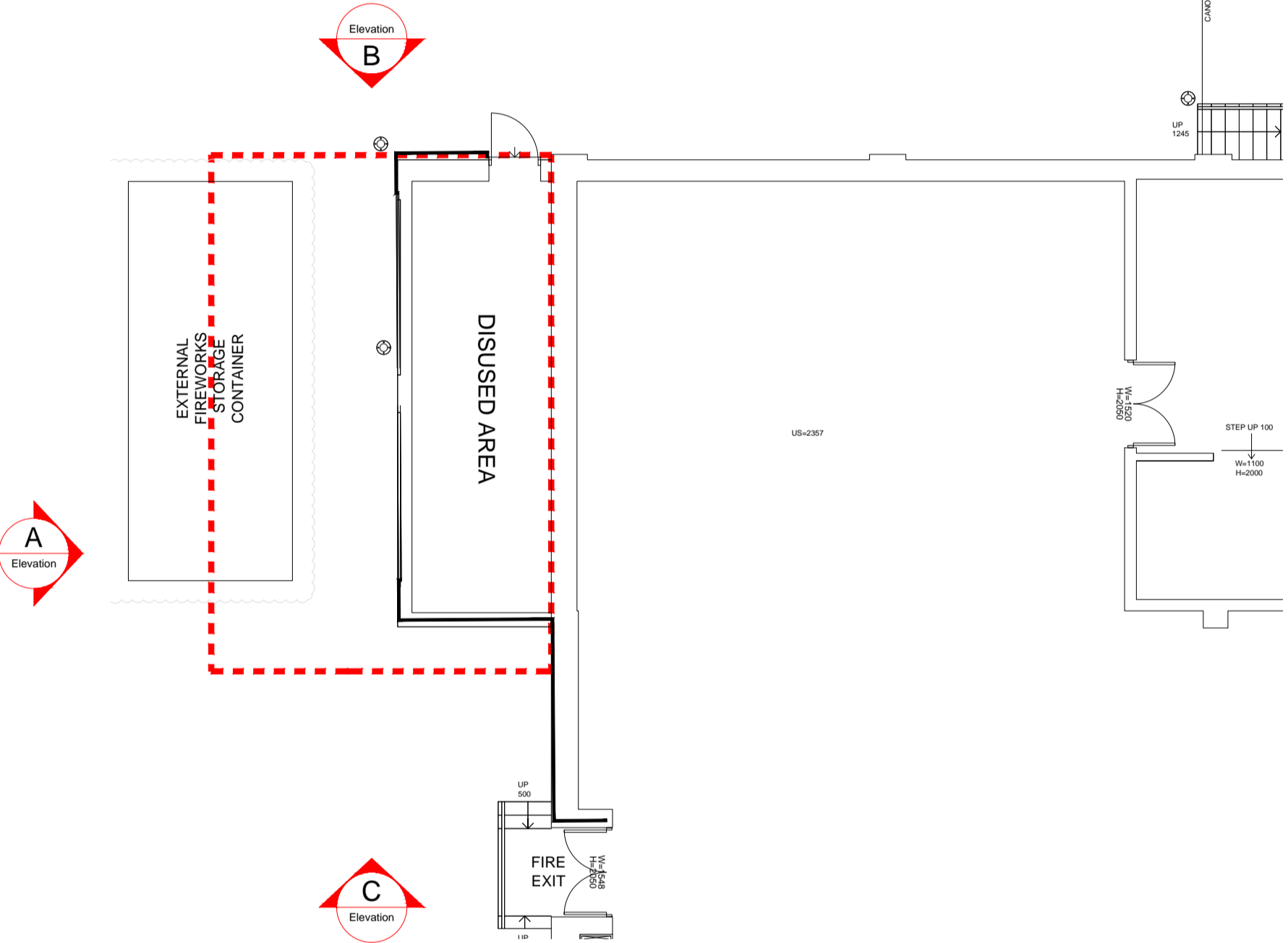
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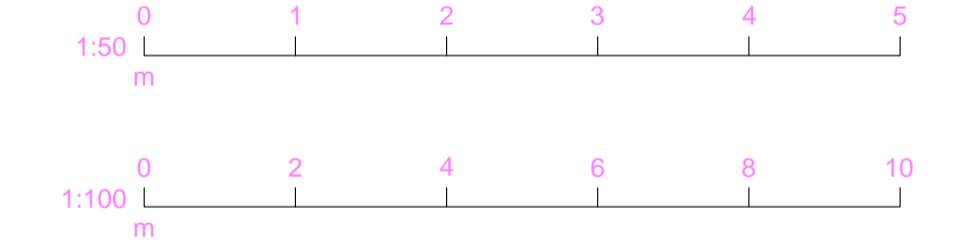
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3 Existing Elevation C
1:50 @ A1



5 Existing Plan
1:100 @ A1



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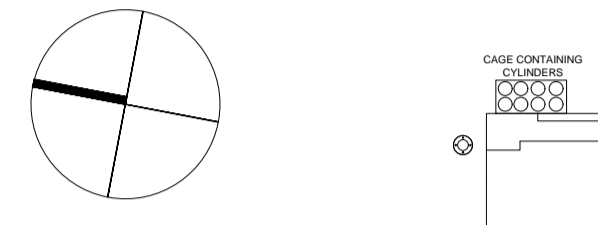


3 View 1



4 View 2

----- Application Boundary



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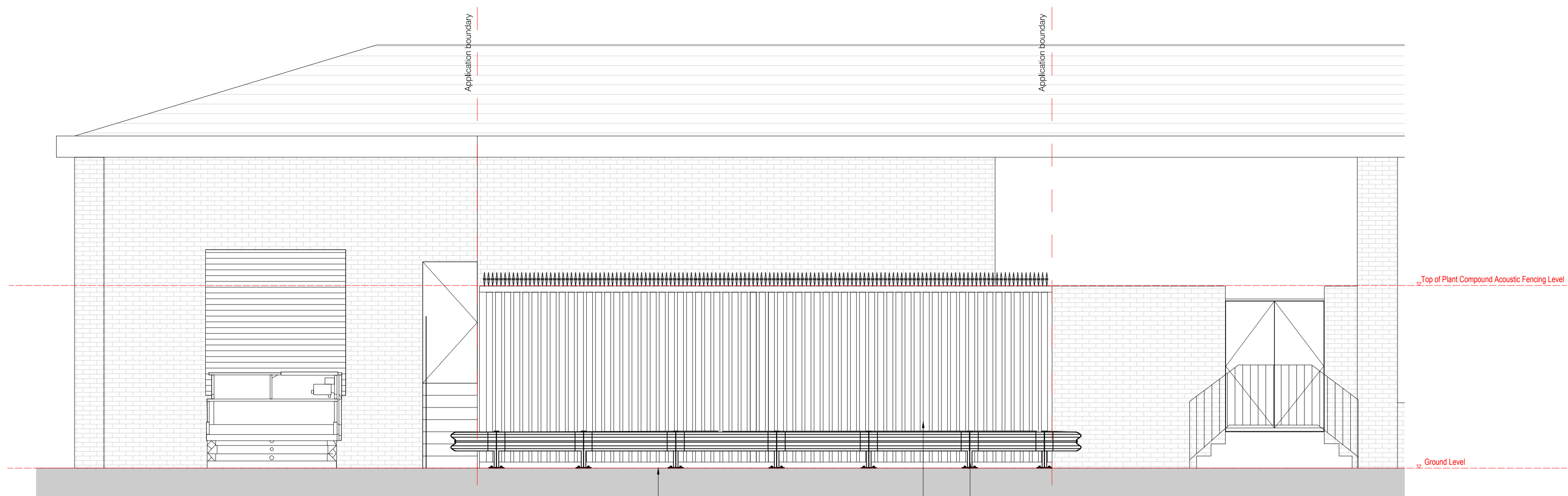
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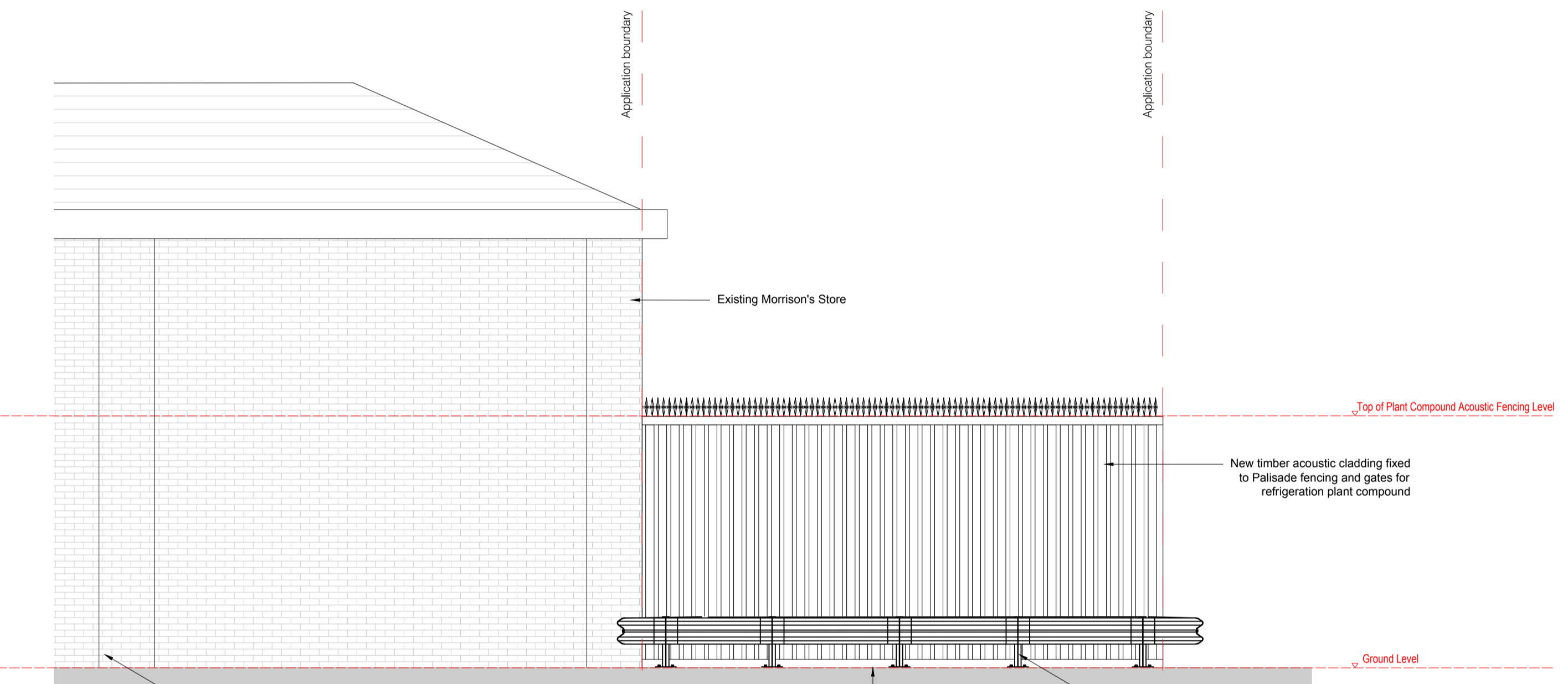
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| CLIENT | |
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| PROJECT | |
| Refrigeration Replacement 2020 | |
| #281 WMS Southend Eastwood | |
| DRAWING | |
| General Arrangement | |
| Existing Plant Enclosure Elevation | |
| SCALE @ A1 | CHECKED |
| 1:50 | LA FH |
| CREATION DATE | REVISION |
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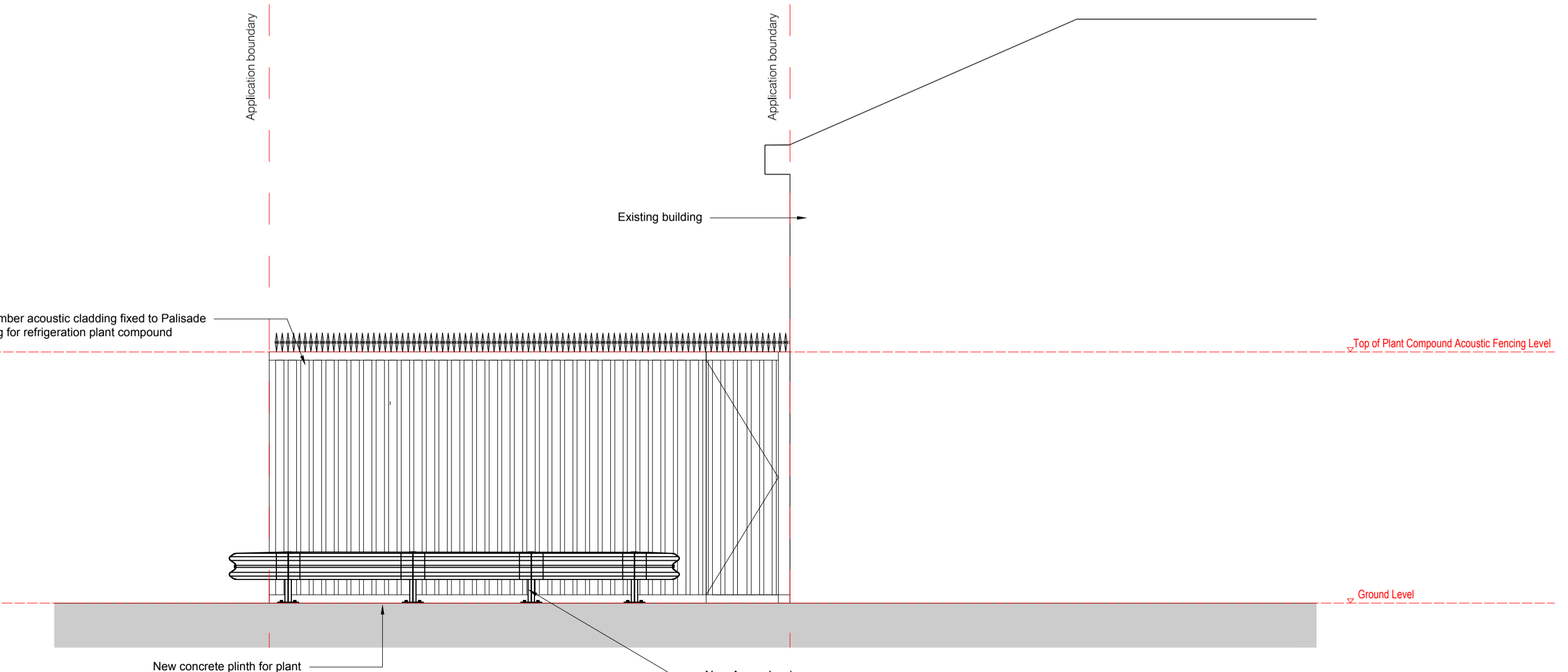
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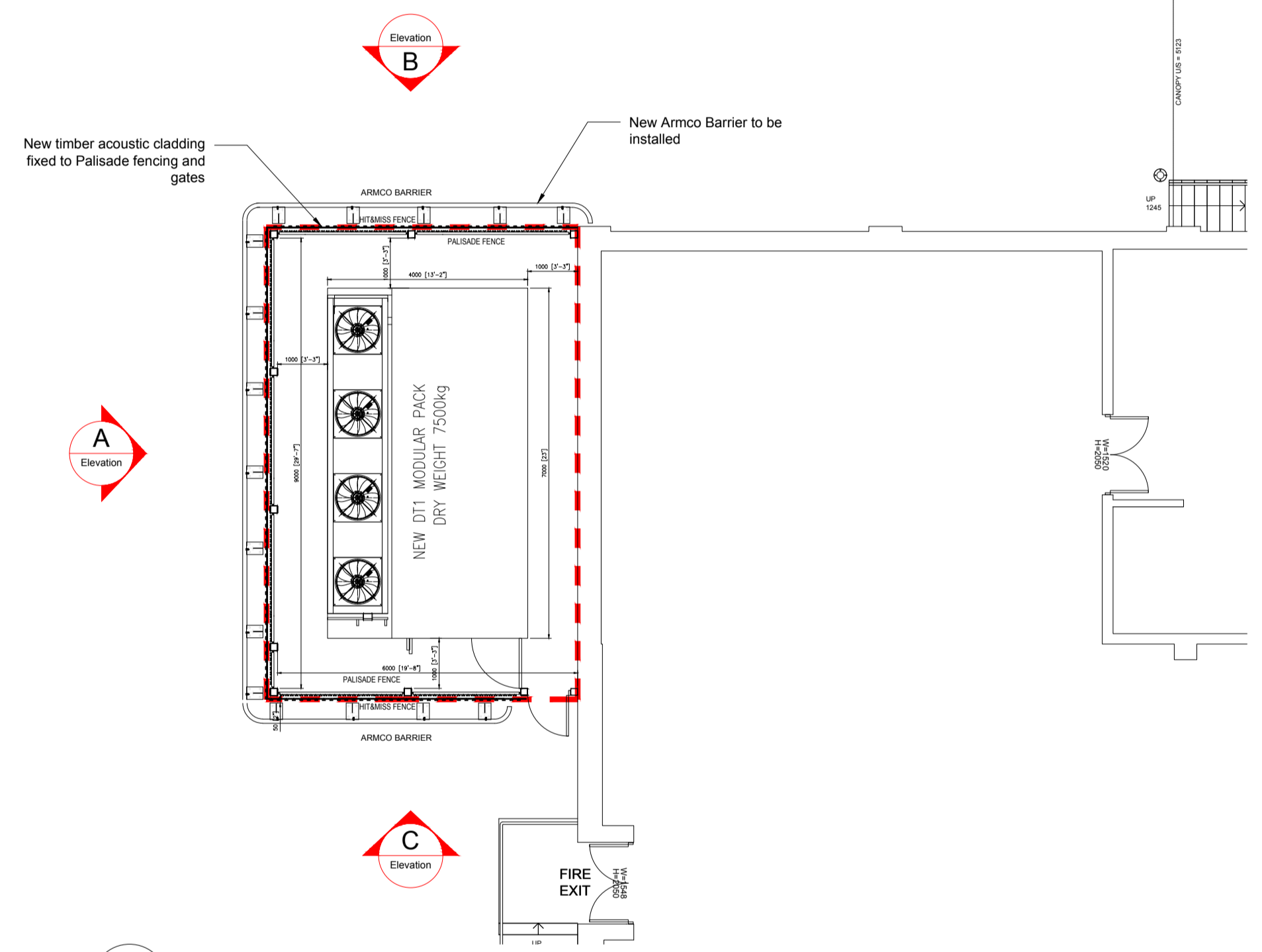
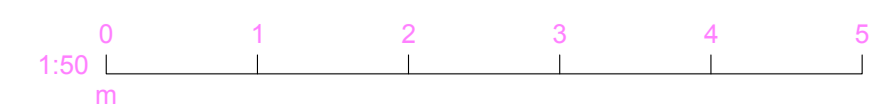
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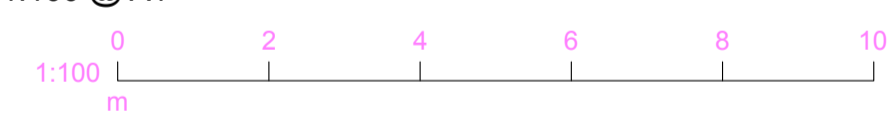
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1:50 @ A1



3 Proposed Elevation C
1:50 @ A1



4 Proposed Plan
1:100 @ A1



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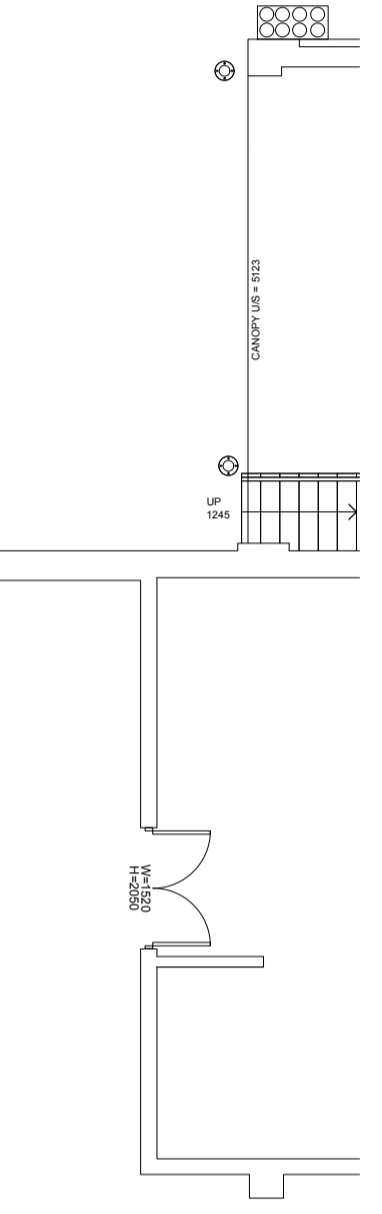
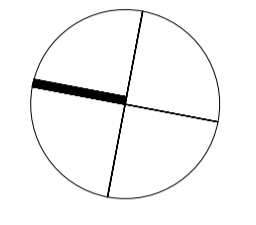
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--- Proposed Refrigeration Boundary Line



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| Refrigeration Replacement 2020 | |
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| DRAWING | |
| General Arrangement | |
| Proposed Plant Enclosure Elevation | |
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Morrisons Western Approaches Eastwood

20/00580/FUL

Application site



Application site and view to south and site boundary



View to west



View north to Western Approaches

139



Existing plant to be removed

